

# BAXTER



"A Growing Community"

## Planning and Zoning Commission Agenda November 9, 2016 @ 6:00 p.m.

1. **Call to Order**
2. **Roll Call**
3. **Approval of regular meeting minutes from October 11, 2016**
4. **Commission Comments and Questions.**

**ALL OLD AND NEW BUSINESS ITEMS ARE OPEN FOR PUBLIC COMMENT**

5. **Old Business**  
None

6. **New Business**

- A. **PUBLIC HEARING.** Preliminary and Final Plat of Mills Addition to Baxter, Conditional Use Permits for two principal uses on a single lot and joint access/parking/lighting, Variances for: 1) drive isle and parking setbacks on Design Road, Golf Course Drive, Excelsior Road and Dellwood Drive; 2) structure setbacks from interior lot lines and from Golf Course Drive, Excelsior Road and Dellwood Drive; 3) sign setbacks from interior lot lines and from Design Road and Dellwood Drive; and 4) impervious surface coverage for properties located at 14138, 14086 and 14114 Dellwood Drive (SW1/4 OF SW1/4 SEC 5 EX N 366FT THEREOF AND EX E 388FT THEREOF. SUBJ TO ESMNT OF REC. AND EX PT TO STATE OF MINNESOTA) (N 836FT OF W 330FT OF E 388FT OF SW1/4 OF SW1/4 SEC 5 EX N 366FT THEREOF) (W 330FT OF E 388FT OF SW1/4 OF SW1/4 SEC 5 EX THE N 836FT THEREOF & EX THE S 400FT THEREOF) (S 400FT OF W 330FT OF E 388FT OF SW1/4 OF SW1/4 SEC 5 SUBJ TO AN ESMNT OF REC) (City file 16-28)

Requested by: MFF Mortgage Barrower 3 LLC c/o Davidson Kempner Capital Management LP, 520 Madison Ave. 30<sup>th</sup> Floor, New York, NY 10022

- B. **PUBLIC HEARING.** Conditional Use Permits for two drive through businesses and for joint access/parking to allow the development of two, multi-tenant retail buildings named Northwoods Crossing located at 15146 Dellwood Drive and the adjacent vacant lot to the south (THAT PART OF THE SW ¼ OF SW1/4 OF SECTION 32 DESCRIBED AS FOLLOWS: COMM. AT THE SW CORNER OF THE [complete legal on file at City Hall]) and (PART OF SW1/4 OF SW ¼ SEC. 32 DESCRIBED AS FOLLOWS: COMM. AT THE SW CORNER OF THE SWSW OF SEC. 32 THEN DUE N. ALONG THE [complete legal on file at City Hall]) (City file 16-29)

Requested by: Baratto Brothers Construction, Inc./Bradford Northwoods Holdings, LLC 215 ½ Main St. Cedar Falls, IA 50613

7. **Other Business**
8. **Adjournment**

**PLANNING & ZONING COMMISSION MINUTES**  
**October 11, 2016 - 6:00 p.m.**

**CALL TO ORDER**

The regular meeting of the Planning and Zoning Commission was called to order at 6:00 p.m.

MEMBERS PRESENT: Chair Bob Kinzel, Commissioners Steve Lund, Gwen Carleton, Howie Oswald, Bob Ryan and Council Liaison Steve Barrows

MEMBERS ABSENT: None

STAFF PRESENT: CD Director Doty and Planner Matthew Gindele

OTHERS PRESENT: Jeff Pewitt

**APPROVAL OF MINUTES**

**MOTION** by Commissioner Ryan, seconded by Commissioner Oswald to approve the minutes from the September 13, 2016 meeting as presented. Motion carried unanimously.

**PUBLIC HEARINGS**

*All items under old and new business items will go to City Council on  
October 18, 2016 if not tabled, continued or otherwise noted.*

Chair Kinzel stated that this Commission is advisory and the final approval/denial is given at the City Council Meeting on October 18, 2016 at 7:00 pm.

**OLD BUSINESS**

None

**NEW BUSINESS**

1. PUBLIC HEARING. Conditional Use Permits for grading related to the construction of a new home within a shoreland overlay district and to allow the construction of a detached accessory structure with cumulative square footage greater than 1,800 square feet for property located at 4587 Joler Road, Baxter, MN. (LOT 2, BLOCK 1, JASPER HEIGHTS)  
(City file 16-26)

Requested by: Jeff Pewitt, PO Box 81, Finley, ND 58230.

Chair Kinzel asked Planner Gindele to start with the staff report. Planner Gindele stated this is a request for a conditional use permit (CUP) to construct a 1,152 square foot detached garage in addition to a proposed 882 square foot attached garage and is requesting a conditional use permit (CUP) to allow cumulative accessory building square footage to exceed 1,800 square feet. The applicant is also requesting a CUP to allow grading related to the construction of the accessory structure and the new home within the shoreland zoning district of unnamed lake 18-382.

Planner Gindele reviewed the aerial map, site plan and grading plan, indicating that all setbacks are being met by Baxter code and the DNR regulations. He further explained the location of the detached garage and how it meets the code. There is plenty of screening between this lot and the neighbors, thus another reason for the detached garage location.

Planner Gindele stated that staff is recommending approval of the CUP application with the findings and conditions noted in the draft resolution.

Chair Kinzel confirmed the location with staff via the GIS overlay. Chair Kinzel asked for additional clarification on the grading plan.

Chair Kinzel opened the public hearing.

Mr. Pewitt (the owner/applicant) speaking from the audience, explained the grading plan to the Commission. Chair Kinzil asked if there were any questions or concerns with the conditions, he had no concerns. Mr. Pewitt gave the Commission his background.

Chair Kinzel closed the public hearing.

**Motion** by Commissioner Oswald, second by Commissioner Carleton to recommend the City Council approve the conditional use permit for grading related to the construction of a new home within a shoreland overlay district and to allow the construction of a detached accessory structure with cumulative square footage greater than 1,800 square feet for property located at 4587 Joler Roadas presented by staff in the draft resolution. Motion carried unanimously.

**OTHER BUSINESS**

CD Director Doty informed the Commission that the next meeting in November will have the Dellwood Annexation area on the agenda. He also reminded the Commission that due to elections, the next Planning and Zoning meeting will be Wednesday, November 9, 2016.

**ADJOURNMENT**

**MOTION** by Commissioner Oswald, seconded by Commissioner Ryan to adjourn the meeting at 6:19 p.m. Motion carried unanimously.

Approved by:

Submitted by:

\_\_\_\_\_  
Chair Bob Kinzel

\_\_\_\_\_  
Shanna Newman CD Administrative Assistant



**TO:** Baxter Planning and Zoning Commission

**FROM:** Matthew Gindele, Planner

**REVIEWED BY:** Josh Doty, Community Development Director

**DATE:** November 9, 2016 Planning and Zoning Commission Meeting

**REQUEST:** **PUBLIC HEARING.** Preliminary and Final Plat of Mills Addition to Baxter, Conditional Use Permits for two principal uses on a single lot and joint access/parking/lighting, Variances for: 1) drive isle and parking setbacks on Design Road, Golf Course Drive, Excelsior Road and Dellwood Drive; 2) structure setbacks from interior lot lines and from Golf Course Drive, Excelsior Road and Dellwood Drive; and 3) impervious surface coverage for properties located at 14138, 14086 and 14114 Dellwood Drive

**APPLICANT:** MFF Mortgage Borrower 3, LLC

**ZONING:** C-2, Regional Commercial

**1. Application Request**

The applicant has submitted a plat application to combine four lots into two lots effectively creating one legal lot for Mills GM and one legal lot for Mills Fleet Farm and Mills Gas Mart. Due to the pre-existing nonconformities on site, the applicant is also requesting multiple variances and conditional use permits. The additional requested planning applications include variances for drive isle/parking setbacks, structure setbacks and impervious surface coverage and conditional use permits for multiple principal uses on one lot and joint access/parking/lighting. This is a "paper plat" only, as no physical alterations to the property are proposed with this application.

**2. Context**

Adjacent Land Use and Zoning

	Adjacent Land Use	Zoning
North	Design Road/Commercial	ROW/C-2
East	Golf Course Road/Low Density Residential/Office	ROW/C-2/O
South	Excelsior Road/ Commercial	ROW/C-2
West	Dellwood Drive/TH371/Commercial	ROW/C-2

*Characteristics of the Site*

The subject property currently consists of four separate lots of record that are fully developed with a retail store, an auto-sales store, a gas station, outdoor storage area, accessory structures and parking area (landscaping and parking lot lighting are also included). The property is developed to approximately 96 percent impervious surface coverage with a flat topography.

### **3. Analysis of Request**

#### Preliminary & Final Plat

Staff has reviewed the application for consistency with City Code requirements, as well as City policies.

The City's discretion in approving or denying a preliminary and final plat is limited to whether or not the proposed plat meets the standards outlined in the City Code. If it meets these standards, the City must approve the plat.

The C-2, Regional Commercial zoning district requires a minimum lot area of 20,000 square feet and a minimum lot width of 120 feet for corner lots. The plat includes a lot that would be 386,370 square feet and 578 feet wide, which meets the minimum requirements.

#### *Park Dedication*

Staff notes that according to City records, park dedication for the subject property has not been paid. The City's policy is to collect park dedication at time of platting. However, it has been the practice in Baxter to not collect park dedication on lots with existing structures that will remain and therefore, no park dedication is required with this plat.

#### *Traffic*

Staff notes that no alterations to the current development pattern of the site are proposed and therefore, the City does not anticipate any changes to the current traffic pattern surrounding the site.

#### *Right-of-Way*

Staff is recommending a required dedication of seven feet of right-of-way along Golf Course Drive and Excelsior Road. Additionally, staff is recommending that an existing ROW easement along Dellwood be platted as ROW. The dedication of the required ROW will place several structures within the required setback as well as place several uses of the property within the ROW itself. Staff has added conditions to the resolution requiring the right-of-way dedication as well as conditions requiring the submission and recording of limited use agreements for parking/drive isles, lighting, fencing and landscaping that will be in the ROW due to the dedication of the extra ROW.

#### *Access*

Staff notes that no alterations to the current development pattern of the site are proposed and that all current accesses are proposed to remain.

#### Conditional Use Permits

In conjunction with the requested plat application, the applicant is requesting CUPs for joint access/parking/lighting along the new interior lot line between Lots 1 and 2 and a CUP to allow two principal uses on Lot 1 of the proposed plat.

## Joint Access/Parking/Lighting

The new configuration of the proposed plat places a lot line through the center of the parking/drive area on the west side of the subject property between Mills GM and Mills Fleet Farm; this new lot line is also drawn through the center of a row of parking lot lighting. Staff has added a condition to the approving resolution that the applicant submits a joint access/parking/lighting agreement to be recorded with Crow Wing County.

## Multiple Principal Uses on a Single Lot (Lot 1)

There are currently three principal uses on the sum of the subject property: a gas station, a retail store and an auto-sales store. The proposed configuration of the lots separates the auto-sales store from the others and leaves the gas station and the retail store on their own separate lot. The applicant is requesting a CUP to allow the gas station and retail store on the same lot.

Staff has also reviewed the request against the general CUP standards. Section 10-7-4 of the Zoning Ordinance defines the criteria for reviewing and processing a conditional use permit (CUP). Specifically:

- A. *Excessive Burden: The use shall not create an excessive burden on existing parks, schools, streets and other public facilities and utilities which serve or are proposed to serve the area.*

The proposed use will not create a burden on existing parks, schools, streets and other public facilities because the subject property is already fully developed and the proposed uses are pre-existing uses.

- B. *Compatibility: The use shall be compatible with adjacent land uses so that existing uses will not be depreciated in value and there will be no deterrence to development of vacant land.*

The use is compatible with adjacent commercial and office land uses and will not depreciate adjacent property value.

- C. *Appearance: The use shall have an appearance that will not have an adverse affect (sic) upon adjacent properties.*

The uses will not have an adverse effect on adjacent properties because they are consistent with the surrounding area.

- D. *Overall Needs: The use, in the opinion of the planning and zoning commission shall be reasonably related to the overall needs of the city and to the existing land use.*

The subject property is zoned C-2 with commercial retail uses. The uses are consistent with the city's land use plan for the property and meets overall needs.

- E. *Consistent With Purpose: The use shall be consistent with the purposes of this title and purposes of the zoning district in which the applicant intends to locate the proposed use.*

The subject property is zoned C-2 with commercial retail uses. The applicant has submitted a plan that meets the city's zoning requirements with the granting of this CUP as long as all conditions are met.

F. *Comprehensive Plan: The use shall not be in conflict with the comprehensive plan of the city.*

The use is not in conflict with the comprehensive plan of the city.

G. *Traffic: The use will not cause traffic hazard or congestion.*

The use will not cause a traffic hazard or congestion.

H. *Facilities: The use shall have adequate utilities, access roads, drainage and necessary facilities.*

The site has adequate utilities, access, drainage and necessary facilities.

### VariANCES

As previously mentioned, the applicant is requesting several variances in conjunction with the proposed plat including Variances for: 1) drive isle and parking setbacks on Design Road, Golf Course Drive, Excelsior Road and Dellwood Drive; 2) structure setbacks from interior lot lines and from Golf Course Drive; and 3) impervious surface coverage on lots 1 and 2. Some of these variances are for existing legally nonconforming conditions present on the subject property, and some are for conditions that are created by the City's requirement for additional ROW dedication on Dellwood Drive, Golf Course Drive and Excelsior Road.

*Drive Isle and Parking Setbacks Variances* (seven drive isle/parking setback variances are requested)

- A 0-foot setback is proposed from the existing ROW on Design Road on Lot 2 where 10 feet is required
- A 0-foot setback is proposed from the proposed ROW on Golf Course Drive on Lots 1 and 2 where 10 feet is required
- A 0-foot setback is proposed from the proposed ROW on Excelsior Road on Lot 1 and 2 where 10 feet is required
- A 0-foot setback is proposed from the existing ROW on Dellwood Drive on Lots 1 and 2 where 10 feet is required

*Structure Setbacks Variances* (three structure setbacks variance are requested)

- A 3-foot accessory structure setback is proposed from the proposed ROW on Golf Course Drive on Lot 1 where 10 feet is required
- A 32-foot principal structure setback is proposed from the proposed ROW on Golf Course Drive on Lot 1 where 35 feet is required
- A 0-foot accessory structure setback is proposed on Lot 1 from the proposed interior lot line between lots 1 and 2 where 10 feet is required

*Impervious Surface Variances* (two impervious surface variances are requested)

- 97.6 percent impervious surface coverage is proposed for Lot 1 when a maximum of 88 percent is allowed
- 97.8 percent impervious surface coverage is proposed for Lot 2 when a maximum of 88 percent is allowed

Staff notes that there are numerous pre-existing non-conforming signs on the property. Staff's recommendation is that the signs are allowed to remain as pre-existing non-conforming signs, subject to the City's non-conforming regulations.

#### **4. Utility Commission Review**

On November 2, 2016 the Utilities Commission reviewed the application and advised the Planning and Zoning Commission that the Utilities Commission has no concerns with the Mills Addition Plat contingent on the following:

- Dedicate 7 feet of additional right-of-way on Golf Course Drive.
- Dedicate 7 feet of additional right-of-way on Excelsior Road.
- Waive additional right-of-way as per City ordinance on Design Road.
- Require a limited Use Agreement for landscaping, parking lot and signage on Dellwood Drive, Golf Course Drive and Excelsior Road right of ways.
- Require a formal storm water agreement for maintenance and liability on the underground storm water pipe under the existing building and the entire site.
- Dedicate the existing 11-foot roadway and utility easement for road right-of-way on Dellwood Drive.

Staff has added conditions to the draft resolution regarding the above Utilities Commission recommendation.

#### **5. Recommendation**

Staff recommends approval of the preliminary and final plat, conditional use permits, and variances, subject to the findings and conditions in the attached resolution.

#### **Attachments**

1. Draft Resolution Approving the Preliminary and Final Plat
2. Draft Resolution Approving the CUPs and Variances for Lot 1
3. Draft Resolution Approving the CUPs and Variances for Lot 2
4. Site Location Map
5. ALTA Survey of Existing Conditions
6. Preliminary Plat
7. Final Plat
8. Table of Required CUPs/Variances/Limited Use Agreements

**CITY OF BAXTER, MINNESOTA  
RESOLUTION 2016-XX**

**RESOLUTION APPROVING A PRELIMINARY AND FINAL PLAT  
FOR "MILLS ADDITION TO BAXTER" FOR PROPERTY LOCATED AT 14138, 14086 AND  
14114 DELLWOOD DRIVE (CITY FILE NUMBER 2016-28)**

WHEREAS, MFF Mortgage Borrower 3 LLC ("the applicant") has requested approval of a preliminary and final plat on property located at 14138, 14086 and 14114 Dellwood Drive, legally described as follows:

SW1/4 OF SW1/4 SEC 5, TOWNSHIP 133, RANGE 28, EX N 366FT  
THEREOF AND EX E 388FT THEREOF. SUBJ TO ESMNT OF REC.  
AND EX PT TO STATE OF MINNESOTA.

TOGETHER WITH: N 836FT OF W 330FT OF E 388FT OF SW1/4 SEC 5, TOWNSHIP 133,  
RANGE 28, EX N 366FT THEREOF.

TOGETHER WITH: W 330FT OF E 388FT OF SW1/4 OF SW1/4 SEC 5, TOWNSHIP 133,  
RANGE 28, EX THE N 836FT THEREOF & EX THE S 400FT THEREOF.

TOGETHER WITH: S 400FT OF W 330FT OF E 388FT OF SW1/4 OF SW1/4 SEC 5,  
TOWNSHIP 133, RANGE 28, SUBJ TO AN ESMNT OF REC.

WHEREAS, the Planning and Zoning Commission has reviewed the request at a duly called a Public Hearing on November 9, 2016 and recommends approval, and;

WHEREAS, the City Council considered the Planning and Zoning Commission recommendation at their November 15, 2016 meeting;

NOW, THEREFORE, BE IT HEREBY RESOLVED BY THE CITY COUNCIL OF THE CITY OF BAXTER, MINNESOTA, that it should and hereby does approve the request to approve preliminary and final plat, based on the finding that the standards in Title 11 of the Baxter Subdivision Ordinance are met. Specifically:

1. The subdivision does not land lock or otherwise impair convenient ingress and egress to or from the rear or side of the subject tracts or any adjacent property;
2. The subdivision does not fall within the corridors of any planned or proposed street as shown upon the official map or approved area plans; and
3. The subdivision does not violate any local, state or federally adopted law, ordinance, regulation, plan or policy.

**FURTHER BE IT RESOLVED** that the following conditions of approval shall be met:

1. The approval of the preliminary and final plat is based on the plans received by the city on October 18, 2016, except as may be amended by this resolution.
2. **Prior to Recording the plat**, the applicant shall:
  - a. Establish a 2-hour fire rating for the existing storage rack located between the north walls of the Mills Fleet Farm building and Design Road, or, complete other measures that meet the 2015 Minnesota Building Code standards. Any

alterations to the exterior of the storage rack are subject to the architectural review ordinance. A building permit is required prior to beginning any construction.

- b. Submit plat recording checklist items as reviewed and approved by city staff.
  - c. Amend the legal descriptions on all existing Conditional Use Permits for the subject property.
  - d. All water and sanitary sewer easements be dedicated on the plat.
  - e. Submit for review by the City, and record with Crow Wing County, Limited Use Agreements for:
    - i. Landscaping, parking/drive isle and fencing in the right-of-way of Golf Course Drive.
    - ii. Parking/drive isle/sidewalk, fencing and lighting in the right-or-way of Excelsior Road.
    - iii. Landscaping, parking/drive isle and lighting in the right-of-way of Dellwood Drive.
    - iv. Landscaping in the right-of-way of Design Road
  - f. Submit for review by the City, and record with Crow Wing County, a joint access/parking/drive isle/lighting agreement between Lots 1 and 2.
  - g. Dedicate 7 feet of additional right-of-way on Golf Course Drive and a 10-foot wide Drainage and Utility Easement.
  - h. Dedicate 7 feet of additional right-of-way on Excelsior Road and a 10-foot wide Drainage and Utility Easement except for where existing permanent structures are located.
  - i. Submit for review by the City, and record with Crow Wing County, a formal storm water agreement for maintenance and liability on the underground storm water pipe under the existing building and the entire site.
  - j. Dedicate the existing 11-foot roadway and utility easement for road right-of-way on Dellwood Drive and a 10-foot wide Drainage and Utility Easement except for where existing permanent structures are located.
3. The City shall waive additional right-of-way on Design Road per City ordinance.
  4. The final plat approval shall expire two years from of the date of this approval unless the applicant has recorded the plat or requested an extension in writing.

**Whereupon, said Resolution is hereby declared adopted on this 15<sup>th</sup> day of November, 2016.**

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**Darrel Olson, Mayor**

**ATTEST:**

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**Kelly Steele, City Clerk**

***City Seal***

**CITY OF BAXTER, MINNESOTA  
RESOLUTION 16-xx**

**RESOLUTION APPROVING CONDITIONAL USE PERMITS TO ALLOW TWO PRINCIPAL  
USES ON A LOT AND TO ALLOW JOINT ACCESS/PARKING/LIGHTING/STORMWATER  
SEWER AND VARIANCES FOR 1) DRIVE ISLE AND PARKING SETBACKS; 2)  
STRUCTURE SETBACKS FROM INTERIOR LOT LINES AND FROM RIGHT-OF-WAY; 3)  
IMPERVIOUS SURFACE COVERAGE FOR PROPRTY LOCATED AT 14086 AND 14114  
(CITY FILE NUMBER 16-28)**

WHEREAS, MFF Mortgage Barrower 3 LLC (“the applicant”) has requested approval of conditional use permits and variances for property legally described as follows:

LOT 1, BLOCK 1 MILLS ADDITION TO BAXTER, TOWNSHIP 133, RANGE 28,  
SECTION 5

WHEREAS, the Planning and Zoning Commission has reviewed the request at a duly called Public Hearing on November 9, 2016 and recommends approval, and;

Whereas, the City Council considered the Planning and Zoning Commission recommendation at their November 15, 2016 meeting;

NOW, THEREFORE, BE IT HEREBY RESOLVED BY THE CITY COUNCIL OF THE CITY OF BAXTER, MINNESOTA, that it should and hereby does approve the request for a conditional use permits and variances, subject to the following findings and conditions:

1. The conditional use permit allows for two principal uses to exist on a single lot, in accordance with the application and plans received by the city on October 18, 2016, except as may be amended by this resolution, based on the finding that all applicable conditional use permit standards have been met.
2. The conditional use permit allows for joint access, parking, lighting and stormwater sewer across Lots 1 and 2, Block 1, Mills Addition to Baxter, in accordance with the application and plans received by the city on October 18, 2016, except as may be amended by this resolution, based on the finding that all applicable conditional use permit standards have been met.
3. The variances allow for 1) drive isle and parking setbacks on Golf Course Drive, Excelsior Road and Dellwood Drive; 2) structure setbacks from interior lot lines and from Golf Course Drive; and 3) impervious surface coverage, in accordance with the application received by the city on October 18, 2016, except as may be amended by this resolution, based on the finding that all applicable variance standards have been met. The following variances are approved:

Drive Isle and Parking Setbacks Variances

- A 0-foot setback from the proposed ROW on Golf Course Drive where 10 feet is required
- A 0-foot setback from the proposed ROW on Excelsior Road where 10 feet is required
- A 0-foot setback from the existing ROW on Dellwood Drive where 10 feet is required

Structure Setbacks Variances

- A 3-foot accessory structure setback from the proposed ROW on Golf Course Drive where 10 feet is required
- A 32-foot principal structure setback from the proposed ROW on Golf Course Drive where 35 feet is required
- A 0-foot accessory structure setback from the proposed interior lot line between lots 1 and 2, Block 1, Mills Addition to Baxter where 10 feet is required

Impervious Surface Variances

- 97.6 percent impervious surface coverage when a maximum of 88 percent is allowed
4. Approval of the CUPs and variance are contingent upon approval of the "Mills Addition to Baxter" preliminary and final plat.
  5. Approval shall expire within one year of the date of approval unless the applicant commences the authorized use and completes the required improvements.

**Whereupon, said Resolution is hereby declared adopted on this 15th day of November, 2016**

\_\_\_\_\_  
**Darrel Olson, Mayor**

**ATTEST:**

\_\_\_\_\_  
**Kelly Steele, City Clerk**

***City Seal***

**CITY OF BAXTER, MINNESOTA  
RESOLUTION 16-xx**

**RESOLUTION APPROVING A CONDITIONAL USE PERMIT TO ALLOW JOINT  
ACCESS/PARKING/LIGHTING/STORMWATER SEWER AND VARIANCES FOR 1) DRIVE  
ISLE AND PARKING SETBACKS; 2) STRUCTURE SETBACKS FROM INTERIOR LOT  
LINES AND FROM RIGHT-OF-WAY; 3) IMPERVIOUS SURFACE COVERAGE FOR  
PROPERTY LOCATED AT 14086 AND 14114 (CITY FILE NUMBER 16-28)**

WHEREAS, MFF Mortgage Borrower 3 LLC ("the applicant") has requested approval of conditional use permits and variances for property legally described as follows:

LOT 2, BLOCK 1 MILLS ADDITION TO BAXTER, TOWNSHIP 133, RANGE 28,  
SECTION 5

WHEREAS, the Planning and Zoning Commission has reviewed the request at a duly called Public Hearing on November 9, 2016 and recommends approval, and;

Whereas, the City Council considered the Planning and Zoning Commission recommendation at their November 15, 2016 meeting;

NOW, THEREFORE, BE IT HEREBY RESOLVED BY THE CITY COUNCIL OF THE CITY OF BAXTER, MINNESOTA, that it should and hereby does approve the request for a conditional use permits and variances, subject to the following findings and conditions:

1. The conditional use permit allows for joint access, parking, lighting and stormwater sewer across Lots 1 and 2, Block 1, Mills Addition to Baxter, in accordance with the application and plans received by the city on October 18, 2016, except as may be amended by this resolution, based on the finding that all applicable conditional use permit standards have been met.
2. The variances allow for 1) drive isle and parking setbacks on Design Road, Golf Course Drive, Excelsior Road and Dellwood Drive; and 2) impervious surface coverage, in accordance with the application received by the city on October 18, 2016, except as may be amended by this resolution, based on the finding that all applicable variance standards have been met. The following variances are approved:

**Drive Isle and Parking Setbacks Variances**

- A 0-foot setback from the existing ROW on Design Road where 10 feet is required
- A 0-foot setback from the proposed ROW on Golf Course Drive where 10 feet is required
- A 0-foot setback is proposed from the proposed ROW on Excelsior Road on Lot 1 where 10 feet is required
- A 0-foot setback from the existing ROW on Dellwood Drive where 10 feet is required

Impervious Surface Variances

- 97.8 percent impervious surface coverage when a maximum of 88 percent is allowed
- 3. Approval of the CUPs and variance are contingent upon approval of the "Mills Addition to Baxter" preliminary and final plat.
- 4. Approval shall expire within one year of the date of approval unless the applicant commences the authorized use and completes the required improvements.

**Whereupon, said Resolution is hereby declared adopted on this 15th day of November, 2016**

\_\_\_\_\_  
**Darrel Olson, Mayor**

**ATTEST:**

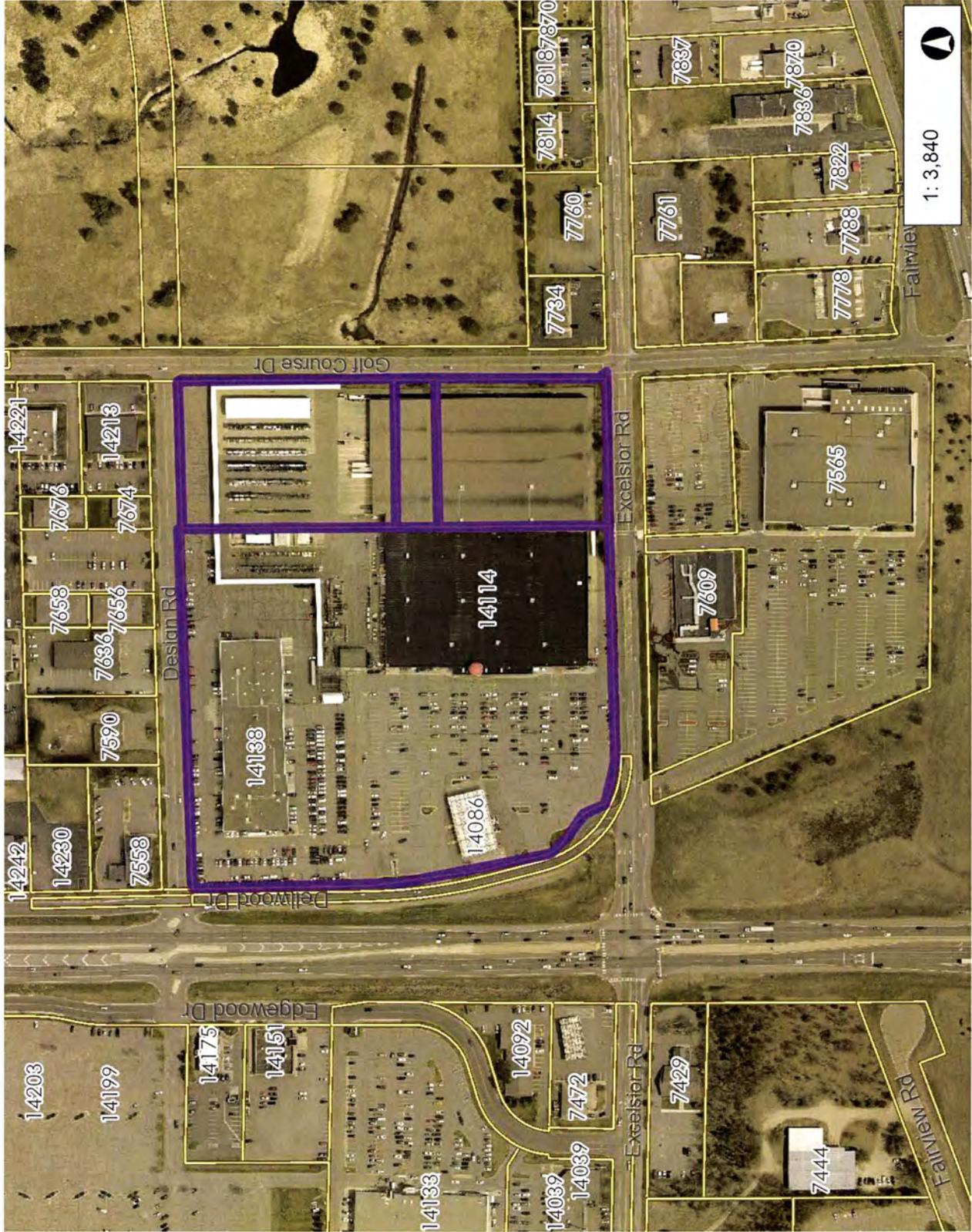
\_\_\_\_\_  
**Kelly Steele, City Clerk**

***City Seal***





# Mills Addition to Baxter



1: 3,840



NAD\_1983\_HARN\_Adj\_MN\_Crow\_Wing\_Feet  
City of Baxter

This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.  
THIS MAP IS NOT TO BE USED FOR NAVIGATION



### Legend

- Streets
- Locations
  - Current
  - Pending
  - Vacant
  - Retired
  - Other
- Parcels
- Land Use Cases
- Encumbrances
- Municipal Boundaries
- Historical Parcels
- 2016 Imagery
  - Red: Band\_1
  - Green: Band\_2
  - Blue: Band\_3

Notes

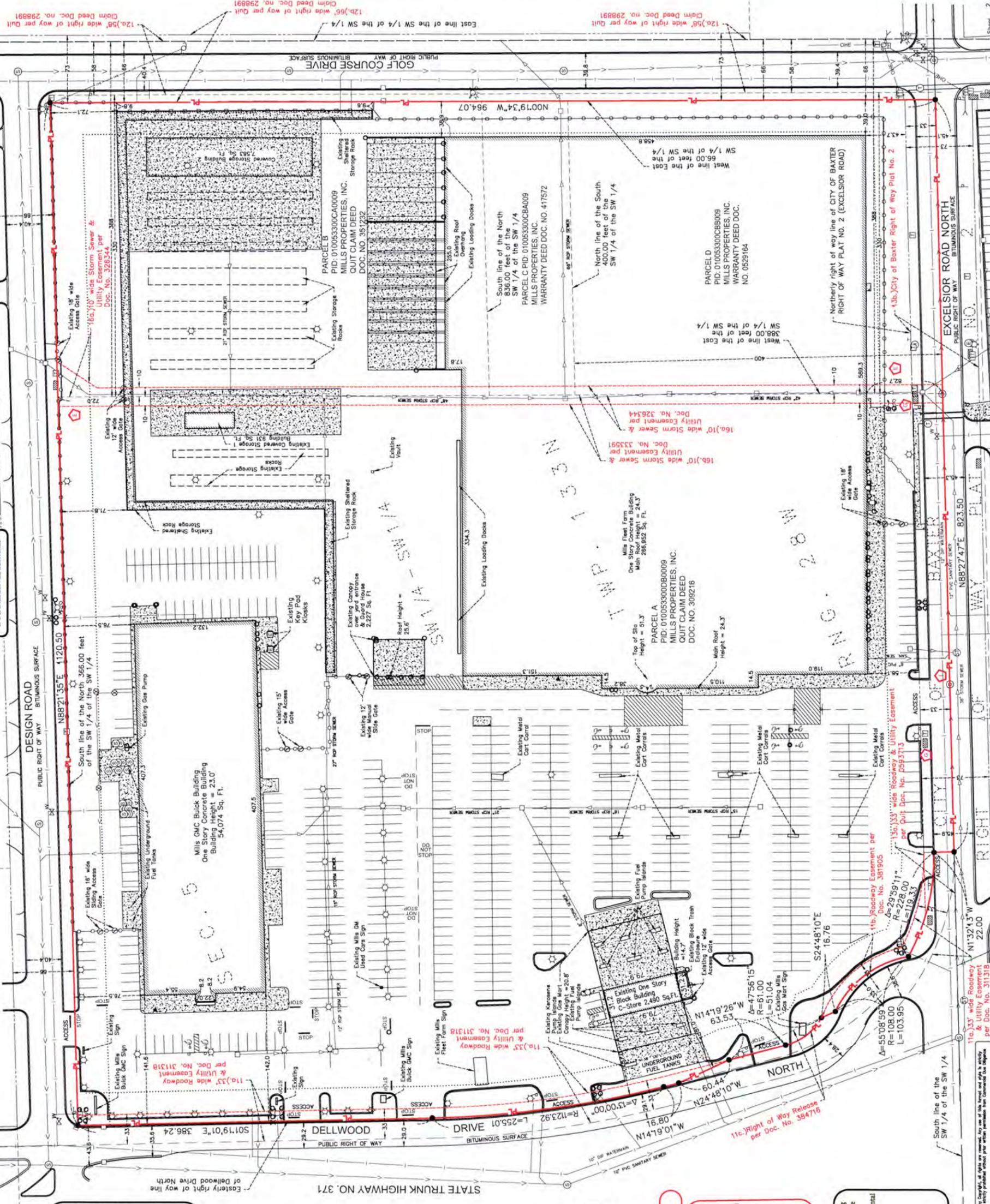
**9 LEGEND**

- = CATCH BASIN
- = GATE VALVE
- = FIRE HYDRANT
- = TELE PEDESTAL
- = GAS METER
- = SANITARY MANHOLE
- = STORM MANHOLE
- = ELEC LIGHT POLE
- = STORM INLET
- = GUARD POST
- = SANITARY CLEANOUT
- = HANDICAP PARKING
- = ELEC TRANSFORMER BOX
- = WETLAND AREA
- = FLAG POLE
- = SIGN - SINGLE POST
- = ELECTRIC HAND HOLE
- = TELEPHONE HAND HOLE
- = FENCE/GATE POST
- = MAILBOX
- = TELEPHONE MANHOLE
- = GUY ANCHOR
- = POWER POLE
- = ELECTRIC KEY PAD
- = DENOTES A FOUND 1/2" x 1/8" IRON PIPE
- = CHAINLINK FENCE
- = WOOD FENCE
- = UNDERGROUND WATER LINE
- = SANITARY SEWER LINE
- = STORM SEWER LINE
- = BOUNDARY LINE
- = LOT LINE
- = RIGHT OF WAY LINE
- = SECTION LINE
- = QUARTER LINE
- = EASEMENT LINE
- = BUILDING SETBACK LINE
- = CONCRETE SURFACE

**18 ALTA/ACSM Land Title Survey**  
This survey prepared in accordance with the "Minimum Standard Detail Requirements for ALTA/ACSM Land Title Surveys" (Effective February 20, 2011)  
This Work Coordinated By:

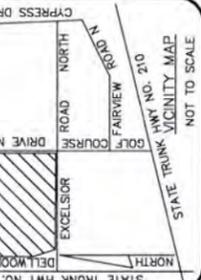


Prepared For:  
Client Ref. No: 721002-418.8  
**20 PROJECT ADDRESS**  
14114 DELLWOOD DR N, BAXTER, MN  
7609 EXCELSIOR RD, N. BAXTER, MN  
Project Name:  
KKR TRANSACTION  
CDS Project Number:  
15-12-12760-030



**19 SURVEY DRAWING**

**16 VICINITY MAP**



**14 BUILDING AREAS**

Main Fleet Farm Building = 366,932 Sq Ft  
Storage Building 1 = 7,583 Sq Ft  
Guard Shack/Yard Entrance = 2,227 Sq Ft  
C-Store Gas Inlet = 2,490 Sq Ft

**15 BUILDING HEIGHTS**

MAIN FLEET FARM BUILDING = 24.3'  
YARD ENTRANCE/GUARD SHACK = 25.6'  
MILLS GMC BUCK BUILDING = 23.0'  
C-STORE ROOF HEIGHT = 14.7'

**17 NORTH ARROW / SCALE**



**7 POSSIBLE ENCROACHMENTS**

- E** Agreement to build Mills Fleet Farm over existing structure (see enclosure per Doc. No. 335591)  
Main Building, storage racks and storage structure begins to encroach easement at 72.0' south of north property line and end the encroachment 82.7' north of the south property line as shown on the survey.
- G** Two electric transformers are without benefit of an easement.
- L** Sanitary sewer manhole is without benefit of an easement.

**WIDSETH SMITH NOLLING**  
Engineering  
Architecture  
Surveying  
Environmental  
Approved CDS Surveyor  
Surveyors Name: Chad M. Conner  
Address: 7804 Industrial Park Road  
Baxter, MN 56425  
Telephone Number: 218-316-3632  
Email: Chad.Conner@wsn.us.com  
WSN Project #: 29382968.001



# MILLS ADDITION TO BAXTER

KNOW ALL PERSONS BY THESE PRESENTS, That MFF Mortgage Borrower 3 LLC, a Delaware Limited Liability Company is the owner of the following described property situated in Crow Wing County, Minnesota:

**LEGAL DESCRIPTION:**  
 Parcel A  
 All of the Southwest Quarter of the Southwest Quarter, Section 5, Township 133, Range 28, except the north 386 feet thereof, and except the east 388 feet thereof, and also except that part thereof conveyed by the Warranty Deed, from Mills Properties, Inc., to the State of Minnesota, dated November 30, 1973, recorded March 27, 1980, in the office of the Crow Wing County Recorder in Book 310 of Deeds, at page 791, as Doc. No. 311318, and also except that part thereof conveyed by the Warranty Deed, from Mills Properties, Inc., to the State of Minnesota, dated September 21, 1987, recorded November 16, 1987, in the office of the Crow Wing County Recorder as Doc. No. 381905, and  
 That part of the Southwest Quarter of the Southwest Quarter of said Section 5 conveyed by the Quit Claim Deed, from the State of Minnesota, to Mills Properties, Inc., dated February 23, 1988, recorded March 3, 1988, in the office of the Crow Wing County Recorder as Doc. No. 384716.

Parcel B  
 The north 636 feet of the west 330 feet of the east 388 feet of the Southwest Quarter of the Southwest Quarter, Section 5, Township 133, Range 28, except the north 386 feet thereof.

Parcel C  
 The west 330 feet of the east 388 feet of the Southwest Quarter of the Southwest Quarter, Section 5, Township 133, Range 28, except the north 836 feet thereof, and except the south 400 feet thereof.

Parcel D  
 The west 330.0 feet of the east 388.00 feet of the south 400.00 feet of the Southwest Quarter of the Southwest Quarter, Section 5, Township 133, Range 28.

Subject to easements, reservations or restrictions of record, if any.  
 Subject to mineral reservations or restrictions of record, if any.

Have caused the same to be surveyed and plotted as MILLS ADDITION TO BAXTER and do hereby dedicate the drainage and utility easements as created by this plat.  
 IN WITNESS WHEREOF, said MFF Mortgage Borrower 3 LLC, a Delaware Limited Liability Company has caused these presents to be signed by its proper officer this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

SIGNED: MFF Mortgage Borrower 3 LLC  
 \_\_\_\_\_  
 State of \_\_\_\_\_  
 County of \_\_\_\_\_  
 This instrument was acknowledged before me on \_\_\_\_\_ by \_\_\_\_\_ of MFF Mortgage Borrower 3 LLC, a Delaware Limited Liability Company.

My Commission Expires: \_\_\_\_\_  
 \_\_\_\_\_  
 PRINT NAME \_\_\_\_\_  
 \_\_\_\_\_  
 County, \_\_\_\_\_  
 Notary Public

I, Chad M. Conner, do hereby certify that this plat was prepared by me or under my direct supervision; that I am a duly Licensed Land Surveyor in the State of Minnesota; that I have personally examined the original and copies of all records, data and labels are correctly designated on this plat; that all monuments depicted on this plat have been, or will be correctly set within one year; that all water boundaries and wet lands, as defined in Minnesota Statutes, Section 505.01, Subd. 3, as of the date of this certificate are shown and labeled on this plat, and all public ways are shown and labeled on this plat.  
 Dated this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

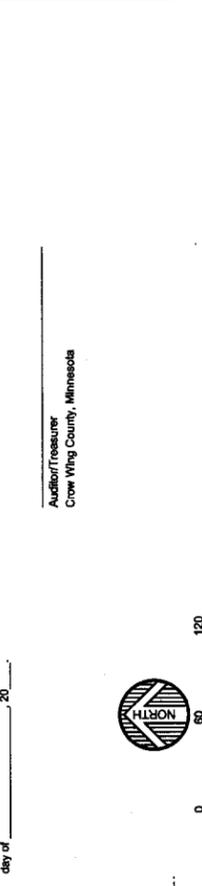
Chad M. Conner, Licensed Land Surveyor  
 Minnesota License Number 41543  
 State of Minnesota  
 County of Crow Wing  
 This instrument was acknowledged before me on \_\_\_\_\_ by Chad M. Conner.

My Commission Expires: \_\_\_\_\_  
 \_\_\_\_\_  
 PRINT NAME \_\_\_\_\_  
 \_\_\_\_\_  
 County, \_\_\_\_\_  
 Notary Public

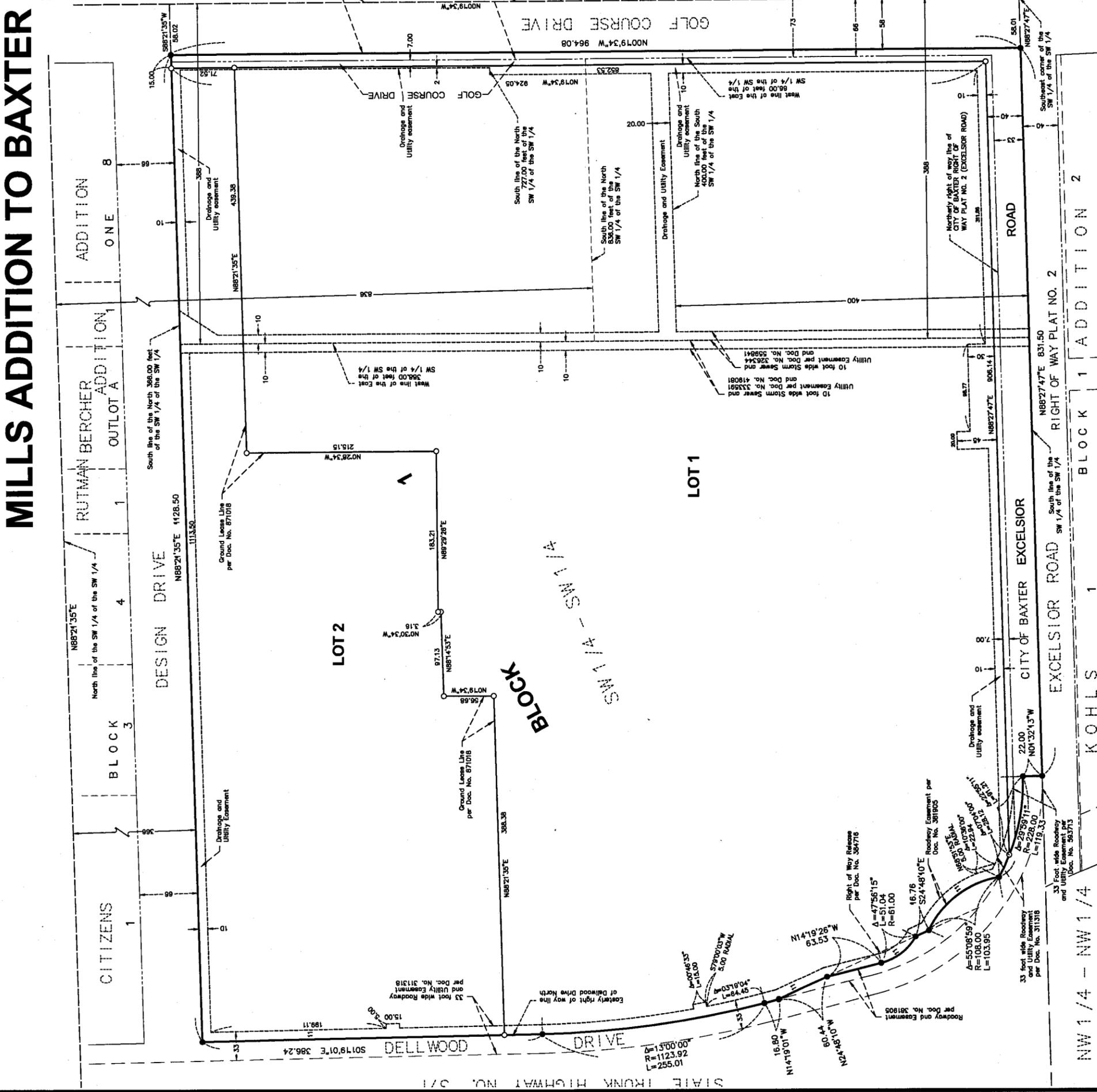
This plat of MILLS ADDITION TO BAXTER was approved and accepted by the City Council of the City of Baxter, Minnesota, at a regular meeting thereof held this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_, and said plat is in compliance with the provisions of the Minnesota Statutes, Section 505.03, Subd. 2.

By: \_\_\_\_\_  
 City Administrator/City Clerk  
 Mayor \_\_\_\_\_  
 Pursuant to Minnesota Statutes, Chapter 272.12, that there are no delinquent taxes on the land heretofore described on this plat and transfer and pursuant to Chapter 505.021, Subd. 5, taxes payable for the year 20\_\_\_\_ on the land heretofore described have been paid this \_\_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

Auditor/Treasurer  
 Crow Wing County, Minnesota



**WIDSETH SMITH NOLTING**  
 Engineering | Architecture | Surveying | Environmental



STATE TRUNK HIGHWAY NO. 571  
 NW 1/4 - NW 1/4  
 KOHLS  
 BLOCK 1  
 ADDITION 2  
 EXCELSIOR ROAD SW 1/4 of the SW 1/4  
 RIGHT OF WAY PLAT NO. 2  
 NBB2747E 831.50  
 CITY OF BAXTER EXCELSIOR  
 DESIGN DRIVE  
 RUTMAN BERCHER ADDITION 1  
 ADDITION ONE  
 BAXTER TOWN CENTER  
 GOLF COURSE DRIVE  
 LOT 1  
 LOT 2  
 BLOCK 3  
 BLOCK 4  
 CITIZENS  
 OUTLOT A  
 OUTLOT H  
 OUTLOT E  
 OUTLOT F

	Lot 2/interior lot line	Lot 1/interior lot line	Design Road	Golf Course Drive (add 7' ROW)	Excelsior Road (plat existing Row and add 7' ROW)	Dellwood Drive (plat existing ROW easement)
<b>Variances</b>						
Drive isle/parking lot	0	0	X (0-foot/Lot 2)	X (0-foot/both lots)	X (0-foot/Lot 1)	X (0-foot/both lots)
Structure setback	0	X (storage rack)	0	X (storage rack and building)		
Impervious surface	X	X	0	0	0	0
Signs						
Joint						
Access/parking/lighting	X	X	0	0	0	0
Principal on 1 lot	0	X	0	0	0	0
<b>Limited Use Agreements</b>						
Landscaping	0	0	X	X	0	X
Parking/drive isle	0	0	0	X	X (and sidewalk)	X
Structure	0	0	0	X (fence)	X (fence)	0
Lighting	0	0	0	0	X	X




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**TO:** Baxter Planning and Zoning Commission

**FROM:** Josh Doty, Community Development Director

**DATE:** November 4, 2016 for the November 9, 2016 Planning and Zoning Commission Meeting

**REQUEST:** **PUBLIC HEARING. Conditional Use Permits** for two drive through businesses and a **Conditional Use Permit** for joint access/parking to allow the development of two, multi-tenant retail buildings named Northwoods Crossing located at 15146 Dellwood Drive and the adjacent vacant lot to the south (city file number 16-29)

**APPLICANT:** Baratto Brothers Construction, Inc.

**ZONING:** C-2, Regional Commercial

**LEGAL:** (THAT PART OF THE SW ¼ OF SW1/4 OF SECTION 32 DESCRIBED AS FOLLOWS: COMM. AT THE SW CORNER OF THE [complete legal on file at City Hall]) and (PART OF SW1/4 OF SW ¼ SEC. 32 DESCRIBED AS FOLLOWS: COMM. AT THE SW CORNER OF THE SWSW OF SEC. 32 THEN DUE N. ALONG THE [complete legal on file at City Hall])

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## 1. Application Request

The applicant is requesting approval of the following applications to construct two 6,000-square foot, multi-tenant retail buildings named Northwoods Crossing at 15146 Dellwood Drive and the adjacent vacant lot to the south:

- 1) Conditional Use Permits to allow two drive through businesses, and
- 2) Conditional Use Permit for joint parking/access.

The developer is proposing to construct two, 6,000-square foot buildings on the site. Each building is showing three tenant spaces. There is also a drive through for each of the buildings. Although the lots would each have their own building, the sites would be connected with shared drive lanes and parking. The property at 15146 Dellwood Drive has an existing 1,000-square foot building (Wells Fargo Bank), which would be removed to allow the construction of the new retail development.

## 2. Context

### Adjacent Land Use and Zoning

	Adjacent Land Use	Zoning
North	Commercial Retail Buildings	C-2, Regional Commercial
East	Commercial Office Buildings	C-2, Regional Commercial
South	Commercial Office Buildings	C-2, Regional Commercial
West	Dellwood Drive/HWY 371	C-2, Regional Commercial

Natural Characteristics of the Site

The site has little topography with a stand of trees the East side of the site. The site does not have any wetland and the site is not located in a shoreland overlay district or floodplain. The north lot is currently developed with a bank with a drive through and the south lot is vacant.

Architectural Commission Review

The applicant has submitted an architectural review application including colored building elevations and sample materials showing compliance with the architectural review requirements, with one exception. The exterior materials are made of composite material that requires review by the Architectural Review Commission and approval by the City Council. On October 28, the Architectural Review Commission reviewed the request and recommended approval. The colored elevations are attached to this report.

Utilities Commission Review

The Utilities Commission reviewed the application on November 2, 2016 and moved to advise the Planning and Zoning Commission that the Utilities Commission has no concerns with the Conditional Use Permits for Northwoods Crossing contingent on the following:

- Require a Developer’s Agreement for the watermain installation be in place before the building permit is issued.
- Require a 100 year storm water design requirements for both lots.
- Require a reconfiguration of the T intersections on the north end of the site.

Site Plan Context

*Access*

The subject site currently has one access point to Dellwood Drive in the southwest corner of the south property and a second access point to the Menards private driveway on the northwest portion of the north property. The northerly access would line up with an existing Culvers drive aisle to the north. Staff notes that the proposed northerly access requires approval by Menards, as they own the private drive. Additionally, staff notes that Menards must be a party to the Conditional Use Permit for joint access. Staff has added conditions to the approving resolution requiring that Menards approves the access and is party to the Conditional Use Permit for joint access.

*Pedestrian Orientation*

The site plan includes sidewalks on all sides of the buildings, except for the northerly sides of the buildings, which has the drive through lanes. Additionally, the applicant is providing a sidewalk connection to Dellwood Drive from the site. Staff has added conditions to the resolution that the applicant provides Pedestrian ramps at the sidewalk connection to Dellwood Drive and at the four

sidewalk connections between the buildings. Staff is also requesting one bike rack on each property.

*Trash Enclosures*

The site plan indicates a trash enclosure at the end of the south access drive isle in direct view from Dellwood Drive and HWY 371. Staff has included a condition to the approving resolution that the trash enclosure be moved to the south or north out of a direct line of sight from Dellwood Drive and HWY 371.

A site plan review for the items listed in the table below is not part of the review for this application. The site plan review will occur prior to issuance of a building permit. However, staff has included the information to show that it appears that the site can be developed in compliance with ordinance requirements.

	<b>Ordinance Requirement</b>	<b>Proposed</b>	<b>Meets/Exceeds Requirements</b>
<b>Building Setbacks</b>	35-foot front setback 10-foot side setback 30-foot rear setback	80 feet 10 feet 140 feet	Yes Yes Yes
<b>Parking &amp; Drive Aisle Setbacks</b>	10-foot setback	10 feet	Yes
<b>Parking</b>	66 spaces (Restaurant 1 stall per 2.5 seats) (Retail 1 stall per 250 sq. ft.)	66 spaces (125 restaurant seats) (4,000 sq. ft. retail)	Yes
<b>Parking Stall Dimensions</b>	10 feet by 18 or 20 feet	10 feet by 18 or 20 feet	Yes
<b>Drive Aisle Dimensions</b>	24 feet (two way) 16 feet (one way)	24 feet 16 feet	Yes Yes
<b>Parking Lot Sidewalks</b>	Sidewalk Connections	Sidewalk Connections	Yes
<b>Building Height</b>	45 feet	22 feet to top of parapet	Yes
<b>Structural Coverage</b>	50 percent	12.8 percent (north) 13.3 percent (south)	Yes Yes
<b>Impervious Surface</b>	88 percent (non-shoreland)	65.9 percent (north) 70.4 percent (south)	Yes Yes
<b>Landscaping</b>			
Number of Trees	12 trees	20 trees + 8 existing	Yes
Size of Trees	2.5-inch and 6 foot	2-inch and 6-foot	No*
Ornamental Substitute	50 percent maximum	Over 50 percent	No*
Priority Placement of Trees	Dellwood Drive	Add trees-Dellwood	No*
Number of Shrubs	27 shrubs	27 shrubs	Yes
Size of Shrubs	3 gallon	3 gallon	Yes
Tree Species	Approved Trees List	Yes	Yes
Percentage of Coniferous	30 % of required trees	6 coniferous	Yes
<b>Parking Lot Islands</b>	One island per 10 stalls One Overstory Tree Per Island	One required island provided – Overstory deciduous tree not provided in island	No*

\*conditions added to resolution

### 3. Analysis of Conditional Use Permits

#### Joint Parking/Access CUP

The applicant is requesting a CUP to allow joint parking/access between the subject properties and to allow joint access with the Menards property to the north. The zoning ordinance permits the typical 10-foot side setback for parking and drive aisles to be reduced to zero to allow joint driveway and parking with approval of a conditional use permit, provided that:

- Driveways and parking areas are designed to accommodate a safe traffic pattern
- Design requirements are met.
- An access master plan is submitted
- A cross-access easement be submitted for approval by the city attorney and recorded.

The attached site plan identifies joint drive aisles and parking on the subject properties. Staff finds that the site plan submitted with this application provides adequate information to be considered an access master plan. Staff has added a condition to the approving resolution that the applicant shall submit a cross access easement for approval by the city attorney and that the easement is recorded with the subject two properties and the Menards property.

#### Drive Through CUPs

The zoning ordinance states that drive through business is allowed subject to the following standards:

- The business shall be located on a site with direct access to a minor arterial street, collector or service road.
- Drive-Through Lanes: Drive-through or drive-in lanes are not allowed between the building and a lot line that faces a public street. This does not pertain to driveways.
- Drive-through facilities, including, but not limited to, the service windows and stacking spaces, shall be separated from residentially zoned or guided property by an arterial or collector street or shall be set back at least two hundred feet (200') from residentially zoned or guided property.
- The public address or order system shall not be audible from any adjacent residentially zoned or guided property. Electronic speaker devices, if used, shall not be audible beyond the property being served and shall not be operated between the hours of ten o'clock (10:00) P.M. and seven o'clock (7:00) A.M., unless extended by the city council as part of the conditional use permit.
- Adequate stacking distance shall be provided, which does not interfere with other driving areas, parking spaces, or sidewalks. Stacking spaces shall not interfere with parking spaces or traffic circulation.
- Businesses with one drive-through lane shall provide stacking space for at least ten (10) vehicles, as measured from and including the last pick up station, window, or the like.
- The applicant shall demonstrate that such use will not significantly lower the existing level of service on streets and intersections. The city may require a traffic study to be prepared.
- Screening shall be provided of automobile headlights in the drive-through lane to adjacent properties. Such screening shall be at least three feet (3') in height and fully opaque, consisting of a wall, fence, dense vegetation, berm, or grade change.
- A bypass lane shall be provided for each drive-through use, allowing cars to leave the drive-through lane from the stacking area.

Staff has added a condition to the approving resolution requiring compliance with the City's drive through requirements. Staff notes the proposed drive through plan meets the majority of the above requirements. Staff notes that additional plantings are needed to meet the screening requirements. Staff finds that the plan meets the stacking and escape lane requirements.

### *Traffic Study*

The attached traffic study provides site plan review comments related to the sites drive through lanes. Specifically, the traffic study states the following:

- The drive through isles shall have separation from the site circulation drive isles. This should be some type of physical barrier, such as a raised concrete median.
- The configuration of the access driveway from the Menards/Culvers Restaurant driveway should be modified to provide a clear lane configuration. This could be accomplished by having the driveway line up directly with the circulation drive isle in front of the building and "Tee" the back circulation isle into it.
- At the locations where the drive through isles cross the circulation drive isles in the back of the building, clear delineation and signing shall be provided so that the circulation isles are not blocked.
- A truck circulation plan, including truck turning radius templates should be provided to insure that trucks can move through the site without impacting parking or drive aisles.

Staff has added conditions to the approving resolution that the applicant submits a revised drive through plan for review and approval by City staff, addressing the above comments from the traffic study. Additionally, pursuant to the traffic study, Dellwood Road shall be re-stripped to provide a center left turn lane from Woida Road to north of the Menards south access/Culvers access with the development.

### *CUP Standards*

Staff has reviewed the request on the general CUP standards. Section 10-7-4 of the Zoning Ordinance defines the criteria for reviewing and processing a conditional use permit (CUP). Specifically:

- A. *Excessive Burden: The use shall not create an excessive burden on existing parks, schools, streets and other public facilities and utilities which serve or are proposed to serve the area.*

The joint driveways/parking and drive through businesses would not create an excessive burden on existing parks, schools, streets and other public facilities.

- B. *Compatibility: The use shall be compatible with adjacent land uses so that existing uses will not be depreciated in value and there will be no deterrence to development of vacant land.*

Joint driveway/parking and drive through lanes are an allowed conditional use in the C-2 district and would not depreciate adjacent properties.

- C. *Appearance: The use shall have an appearance that will not have an adverse affect (sic) upon adjacent properties.*

The joint driveway/parking and drive through lanes would not have an adverse effect on adjacent properties.

*D. Overall Needs: The use, in the opinion of the planning and zoning commission shall be reasonably related to the overall needs of the city and to the existing land use.*

Joint driveway/parking and drive through lanes are commonly a part of commercial development. No outside display is proposed.

*E. Consistent With Purpose: The use shall be consistent with the purposes of this title and purposes of the zoning district in which the applicant intends to locate the proposed use.*

The joint driveway/parking and drive through lanes are consistent with the purposes of the City and the C-2 zoning district. Conditions have been added to the resolution addressing ordinance requirements for drive through screening and traffic circulation site amendments.

*F. Comprehensive Plan: The use shall not be in conflict with the comprehensive plan of the city.*

The use would not be in conflict with the comprehensive plan of the city.

*G. Traffic: The use will not cause traffic hazard or congestion.*

With the conditions in the resolution, the proposed use would not cause a traffic hazard or congestion. The traffic study does indicate that the intersections of TH 371 and Woida Road would operate at below an acceptable level traffic by 2030. The City should work with MnDOT to extend left turn lanes on TH 371 at the Woida Road Intersection.

*H. Facilities: The use shall have adequate utilities, access roads, drainage and necessary facilities.*

The site has adequate utilities, access roads, drainage and necessary facilities.

#### **4. Recommendation**

Staff recommends approval of the conditional use permits, subject to the findings and conditions in the attached resolutions.

#### **Attachments**

1. Draft resolution approving conditional use permits
2. Comment Letter from MnDOT
3. Traffic Study
4. Site Location Map
5. Site Graphics
6. Colored Building Elevations

**CITY OF BAXTER, MINNESOTA  
RESOLUTION 16-\_\_\_\_\_**

**RESOLUTION APPROVING A CONDITIONAL USE PERMITS FOR TWO DRIVE THROUGH  
BUSINESSES AND A CONDITIONAL USE PERMIT FOR JOINT ACCESS/PARKING TO  
ALLOW THE DEVELOPMENT OF TWO, MULTI-TENANT RETAIL BUILDINGS NAMED  
NORTHWOODS CROSSING LOCATED AT 15146 DELLWOOD DRIVE AND THE  
ADJACENT VACANT LOT TO THE SOUTH  
(CITY FILE NUMBER 16-29)**

WHEREAS, Baratto Brothers Construction, Inc. ("the applicant") has requested approval of conditional use permits for property legally described as follows:

That part of SW ¼ of SW ¼ sec. 32 described as follows: comm. at the SW corner of the SWSW sec. 32, then due N. along the W. line of said sec. 32 which line is also the centerline of U.S. Hwy #371, for a distance of 488 ft. to the point of beg, then S. 89 deg. 40 min E. for a distance of 156.5 ft. to an iron monument, then cont. on the same tangent distance of 300 ft. to an iron monument then due N. for a distance of 150 ft. to the iron monument, then N. 89 deg. 40 min W, for a distance of 300 ft. to an iron monument. Then cont. on the same tangent for a distance of 156.5 ft. to the W. line of section 32, then due S. for a distance of 150 ft. to the point of beg. Subject to ROW for Hwy 371.

and

That part of SW ¼ of SW ¼ sec. 32 described as follows: comm. at the SW corner of the SWSW sec. 32, then due N. along the W. line of said sec. 32 which line is also the centerline of U.S. Hwy #371, for a distance of 338 ft. to the point of beg, then S. 89 deg. 40 min E. for a distance of 156.6 ft. to an iron monument, then cont. on the same tangent distance of 300 ft. to an iron monument, then due N. for a distance of 150 ft. to the iron monument, then N. 89 deg. 40 min W, for a distance of 300 ft. to an iron monument. Then cont. on the same tangent for a distance of 156.5 ft. to the W. line of section 32, then due S. for a distance of 150 ft. to the point of beg. Subject to ROW for Hwy 371.

WHEREAS, the Planning and Zoning Commission has reviewed the request at a duly called Public Hearing on November 9, 2016 and recommends approval, and;

Whereas, the City Council considered the Planning and Zoning Commission recommendation at their November 15, 2016 meeting;

NOW, THEREFORE, BE IT HEREBY RESOLVED BY THE CITY COUNCIL OF THE CITY OF BAXTER, MINNESOTA, that it should and hereby does approve the request for conditional use permits, subject to the following findings and conditions:

1. The conditional use permits allow two drive through businesses joint access/parking to allow the development of two, multi-tenant retail buildings named Northwoods Crossing, in accordance with the application and plans received by the city on October 18, 2016 and revised site plan, civil plans, landscaping and lighting plan received on November 4, 2016, except as may be amended by this resolution, based on the finding that all applicable conditional use permit standards have been met.
2. Although not a requirement, the city requests Quit Clam Deeds for the right-of-way on Dellwood Drive.
3. The development plan shall be compliant with the October 25, 2016 traffic study.
4. Dellwood Drive shall be re-stripped pursuant to the October 25, 2016 traffic study.
5. **A building permit is required prior to commencement of construction.**
6. **Prior to the Issuance of a building permit, the applicant shall:**
  - a. Provide a Developer's Agreement for the watermain installation.
  - b. Provide a 100-year storm water design for both lots.
  - c. The drive through isles shall have separation from the site circulation drive isles. This should be some type of physical barrier, such as a raised concrete median. Revise site plan for review and approval by City staff.
  - d. Reconfigure the "T" intersection on the north access location for review and approval by City staff.
  - e. At the locations where the drive through isles cross the circulation drive isles in the back of the building, clear delineation and signing shall be provided so that the circulation isles are not blocked.
  - f. A truck circulation plan, including truck turning radius templates should be provided to insure that trucks can move through the site without impacting parking or drive aisles.
  - g. Provide a site circulation signage plan including "one way" and "do not enter" signage for review and approval by City staff.
  - h. Provide pedestrian ramps meeting State and Federal ADA requirements at the sidewalk connection at Dellwood Drive and at the four sidewalk connections at the drive aisle between the buildings.
  - i. Revise plans to show a bike rack on each property.
  - j. Submit a cross access and parking easement for review and approval by the City Attorney between the subject properties and the Menards property directly north of the site. The cross access and parking easement shall be recorded with the title of the subject properties and the Menards property.
  - k. Submit a revised landscape plan in conformance with the City's landscape ordinance. Specifically, changes are required related to the size of deciduous trees, meeting minimum number of overstory trees, priority placement of trees on Dellwood Drive.
  - l. Submit a revised landscape plan showing an overstory deciduous tree in the required island on the south side of the south property.
  - m. Submit a revised landscape plan providing headlight screening from the drive through lanes.

- n. Submit a revised site plan moving the trash enclosure on the south property so that there is not a direct view of the enclosure from Dellwood Drive.
  - o. All trash and recycling areas shall be housed in an opaque enclosure that is consistent with the primary color of the building. The enclosures shall be reviewed and approved by staff.
  - p. Submit a lighting plan for review and approval by city staff for any ground or building lighting proposed.
  - q. Provide a minimum low floor elevations as required by the City Code.
  - r. Provide a three foot separation from the bottom of ponding to the top of the ground water as required by the MPCA and NPDES permit.
  - s. Provide floor plans and parking calculations showing compliance with the City's minimum parking spaces for the specific uses (retail, restaurant, office, etc.) to ensure that minimum parking is provided on the site.
7. Silt fence shall be installed prior to construction or grading.
8. All ground mechanical equipment shall be fully screened and all rooftop mechanical equipment shall be screened or painted to match the building, according to city requirements.
9. The site or building changes require separate approvals, as required by the City at that time.
10. Signage shall require approval of separate sign permits.
11. Approval shall expire within one year of the date of approval unless the applicant commences the authorized use and completes the required improvements.

**Whereupon, said Resolution is hereby declared adopted on this 15<sup>th</sup> day of November, 2016.**

\_\_\_\_\_  
**Darrel Olson, Mayor**

**ATTEST:**

\_\_\_\_\_  
**Kelly Steele, City Clerk**

***City Seal***



**Minnesota Department of Transportation**

**District 3**

7694 Industrial Park Road  
Baxter, MN 56425

Office Phone: 218-828-5700

Fax: 218-828-5814

Toll Free: 1-800-657-3971

November 1, 2016

Mr. Josh Doty  
Community Development Director  
City of Baxter  
13190 Memorywood Drive  
Baxter, MN 56425

RE: C.S. 1810; R.P. 33.797 (MN 371)  
Northwoods Crossing Development  
Crow Wing County, Minnesota

Dear Mr. Doty:

The Minnesota Department of Transportation (MnDOT) has reviewed the above-referenced development and would like to offer the following comments/recommendations:

1. MnDOT will continue to monitor the traffic situation at the Minnesota State Highway (Hwy) 371/Woida Road intersection and work with the City to determine appropriate solutions if conditions deteriorate to an unsafe level.
2. There does not appear to be discrepancies or encroachments affecting the state right of way requiring action at this time.
3. There shall be no net increase in storm water runoff to the Hwy 371 right of way from said property. Computations of all storm water directed toward the right of way shall be provided to MnDOT. Contact Rich Munsch, District 3 Permits Supervisor, at 218/828-5778 for additional information
4. Since the property abuts MnDOT right of way, a permit is required to perform any work in the right of way. Contact the above-mentioned District 3 Permits Supervisor for additional information.

If you have any further questions or comments, please call me at 218/828-5780.

Sincerely,

Jon Mason  
Senior Planner

mln

ec: MnDOT District 3 Entrance Group

An Equal Opportunity Employer



# Northwoods Crossing Development Traffic Study

For:



City of Baxter  
13190 Memorywood Drive  
Baxter, MN 56425

October 25, 2016

Approved By the Baxter City Council \_\_\_\_\_

Prepared By:

WSB & Associates, Inc.  
15574 Edgewood Drive  
Suite #103  
Baxter, MN 56401  
Phone: (218) 824-3960

**CERTIFICATION**

I hereby certify that this plan, specification or report was prepared by me or under my direct supervision and that I am a duly registered professional engineer under the laws of the State of Minnesota.



---

Charles T. Rickart, P.E.

Date: October 25, 2016

Reg. No. 26082

## TABLE OF CONTENTS

CERTIFICATION .....	1
TABLE OF CONTENTS.....	2
LIST of FIGURES / TABLES.....	2
INTRODUCTION / BACKGROUND .....	3
EXISTING CONDITIONS.....	6
TRAFFIC PROJECTIONS.....	11
TRAFFIC IMPACT ANALYSIS.....	21
SITE PLAN REVIEW.....	26
CONCLUSIONS / RECOMMENDATIONS.....	27

## LIST of FIGURES / TABLES

Figure 1 - Location Map.....	4
Figure 2 - Proposed Site Plan.....	5
Figure 3 - 2016 Existing Conditions (1) .....	9
Figure 4 - 2016 Existing Conditions (2) .....	10
Figure 5 - Projected 2018 No-Build Conditions (1) .....	13
Figure 6 - Projected 2018 No-Build Conditions (2) .....	14
Figure 7 - Projected 2030 No-Build Conditions (1) .....	15
Figure 8 - Projected 2030 No-Build Conditions (2) .....	16
Figure 9 - Projected 2018 Build Conditions (1).....	17
Figure 10 - Projected 2018 Build Conditions (2).....	18
Figure 11 - Projected 2030 Build Conditions (1).....	19
Figure 12 - Projected 2030 Build Conditions (2).....	20
Figure 13 - Level of Service Ranges.....	22
Table 1 - Crash Summary .....	8
Table 2 - Proposed Site Traffic Generation.....	11
Table 3 - 2016 Existing Conditions Level of Service.....	22
Table 4 - Projected No-Build Level of Service .....	23
Table 5 - Projected Build Level of Service.....	24

## INTRODUCTION / BACKGROUND

The purpose of this study is to determine the potential transportation impacts for the development of the old Wells Fargo site, now Northwoods Crossing Development, on Dellwood Drive north of Woida Road, between the Country Inn and Suites Hotel and Culvers Restaurant adjacent to Menards. *Figure 1* shows the proposed project location.

The proposed Northwoods Crossing Development is anticipated to consist of two buildings with a mixture of service retail and restaurant uses. All access from the site will be through driveway connections to Dellwood Drive; one full movement direct access to Dellwood Drive, and one shared driveway with Menards across from the existing Culvers driveway. The proposed site plan is shown on *Figure 2*.

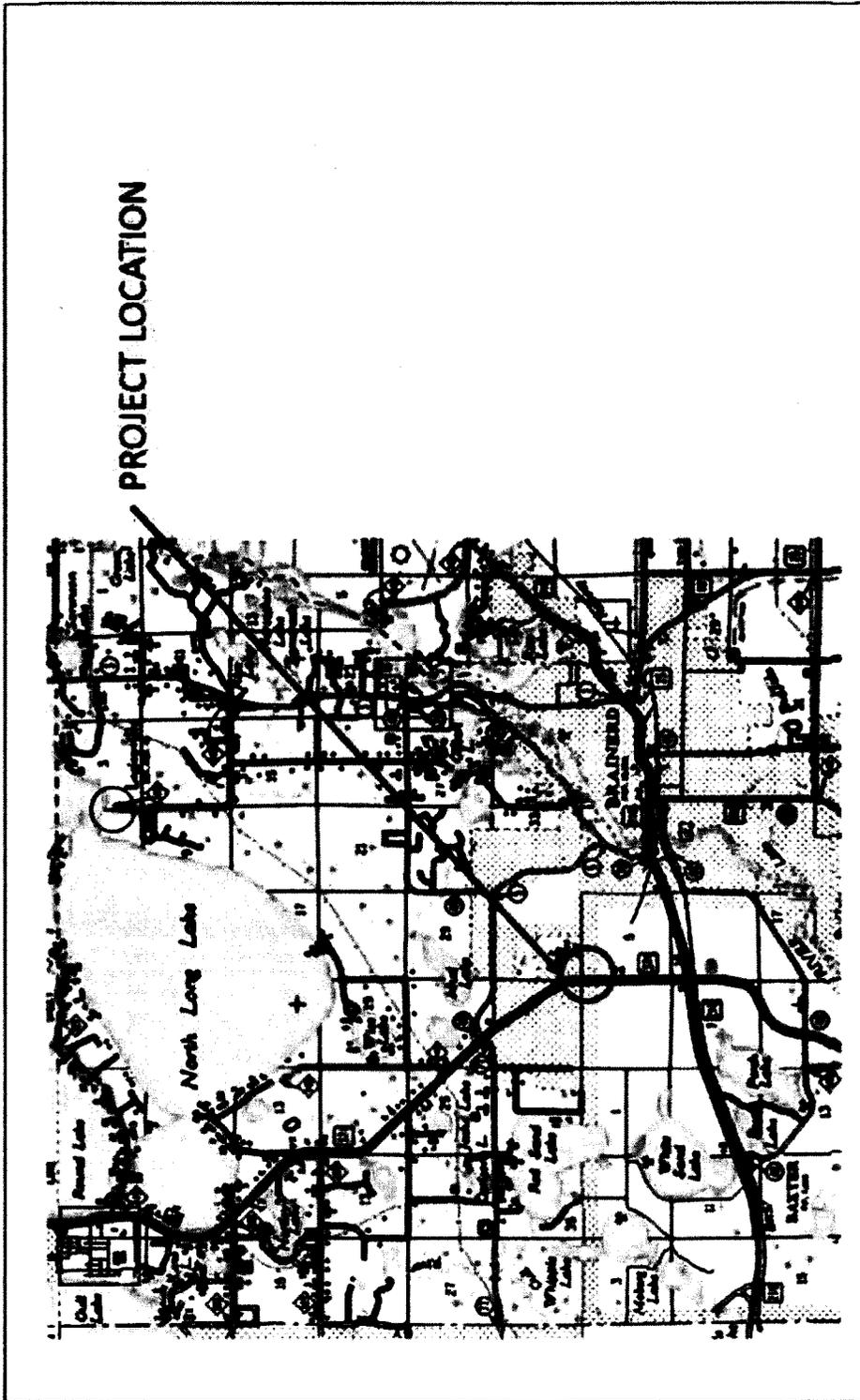
The transportation impacts of the existing and proposed traffic conditions were evaluated at the following intersections:

- Dellwood Drive at Whispering Woods Lane
- Dellwood Drive at Menards north access/Applebee's access
- Dellwood Drive at Menards south access/Culvers access
- Dellwood Drive at Country Inn and Suites access
- Dellwood Drive at Holiday gas station access
- Dellwood Drive at Woida Road
- TH 371 at Woida Road

The Traffic Engineering Review and Analysis in this Study was conducted to determine the impacts the site would have on the area traffic operations, lane geometry/traffic control and access locations.

The following sections of this report outline the findings of this study including:

- Documentation of existing traffic conditions in the area, and;
- Projecting future traffic for the proposed Northwood Crossing Development and anticipated future area development, and;
- Determining lane configuration needs at the primary driveways and intersections along the corridor, and;
- Developing conclusions and recommendations with respect to the existing and future roadway configurations in the corridor.



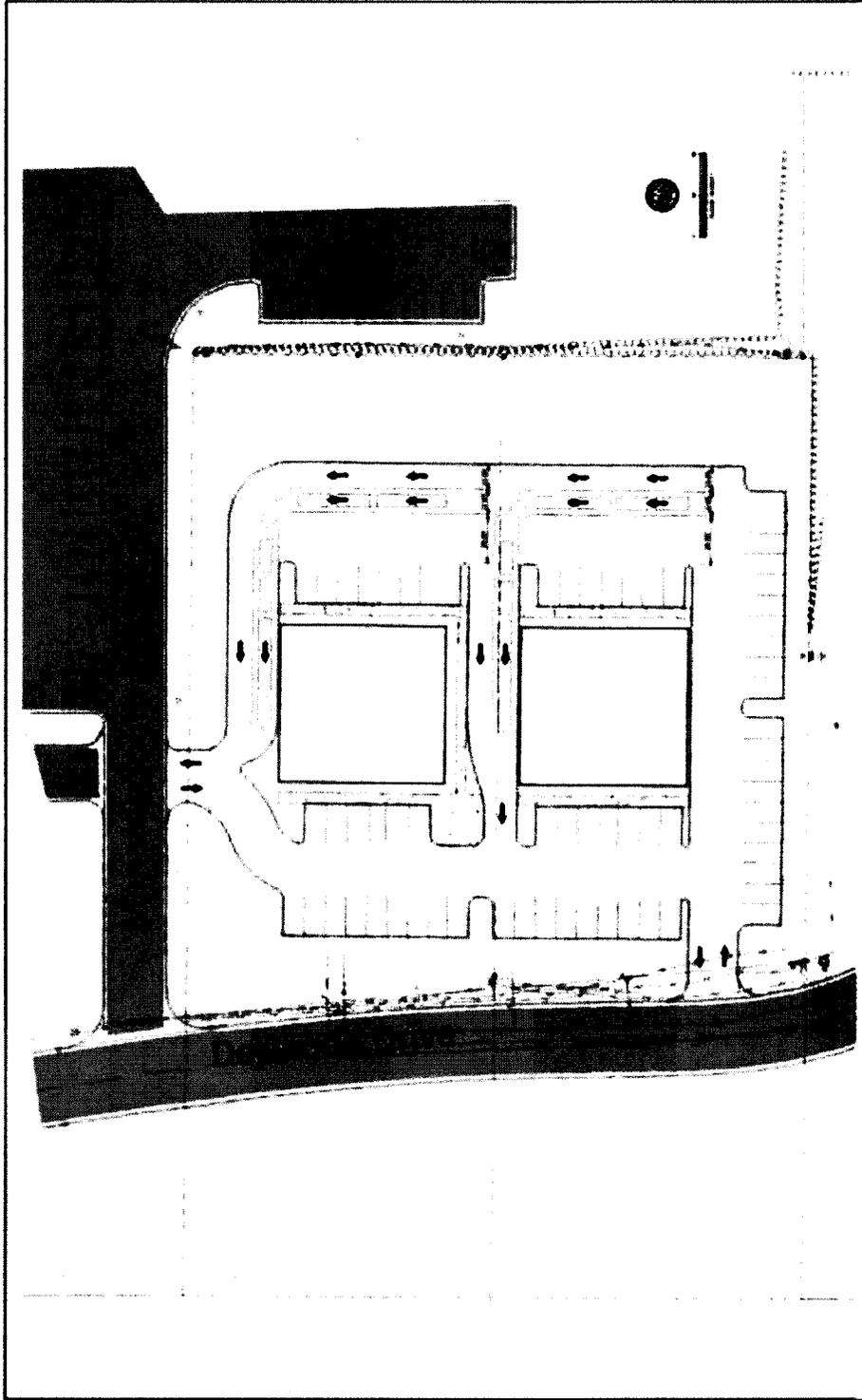
**Figure 1**  
**Project Area**

**Northwoods Crossing Development**  
**City of Baxter, MN**

**BAXTER**



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**Figure 2**  
**Site Development Plan**

**Northwoods Crossing Development**  
**City of Baxter, MN**



## EXISTING CONDITIONS

### A. *Land Use*

The existing land use in the study area consists primarily of commercial and retail uses adjacent to TH 371, Dellwood Drive and Woida Road.

### B. *Roadway Characteristics*

**Woida Road** is an east/west street extending from Garrett Lane on the west to Golf Course Drive on the east. East of Dellwood Drive, Woida Road has a typical 36 foot cross section with two lanes in each direction. Between Dellwood Drive and TH 371, Woida Road widens to an approximate 70 foot cross section to accommodate turn lanes at TH 371 and Dellwood Drive. Traffic signal control is provided at the TH 371 intersection. All other intersections are Stop Sign controlled. A 30mph speed limit is posted on Woida Road east of TH 371.

**Dellwood Drive** is a north/south street extending from Excelsior Road on the south to Wise Road (CR 49) on the north, as the east service road to TH 371. Dellwood Drive has a primary 36 foot two-lane urban cross section with no turn lanes. It provides access to the businesses adjacent to TH 371 with controlled intersections at the major cross streets accessing TH 371 including Woida Road. Dellwood Drive was reconstructed north of Whispering Woods Lane and south of South Menards Access in 2015. A 30mph speed limit is posted on Dellwood Drive.

The lane configurations at each of the study area intersections are as follows:

Dellwood Drive at Whispering Woods Lane – Side Street Stop control

NB Dellwood Dr approaching Whispering Woods Ln - one right/through/left  
SB Dellwood Dr approaching Whispering Woods Ln - one right/through/left  
WB Whispering Woods Ln approaching Dellwood Dr -one right/through/left

Dellwood Drive at Menards North Access/Applebee's – Side Street Stop control

NB Dellwood Dr approaching Menards N. Access - one right/through/left  
SB Dellwood Dr approaching Menards N. Access - one right/through/left  
WB Menards N. Access approaching Dellwood Dr -one right/through/left

Dellwood Drive at Menards South Access/Culvers – Side Street Stop control

NB Dellwood Dr approaching Menards Access - one right/through/left  
SB Dellwood Dr approaching Menards Access - one right/through/left  
WB Menards Access approaching Dellwood Dr - one right/through/left

Dellwood Drive at Country Inn and Suites Access – Side Street Stop control

NB Dellwood Dr approaching Hotel Access - one right/through/left  
SB Dellwood Dr approaching Hotel Access - one right/through/left  
WB Hotel Access approaching Dellwood Dr - one right/through/left

Dellwood Drive at Holiday Gas Access – Side Street Stop control  
NB Dellwood Dr approaching Holiday Access - one right/through/left  
SB Dellwood Dr approaching Holiday Access – one right/through/left  
WB Holiday Access approaching Dellwood Dr – one right/through/left

Dellwood Drive at Woida Road – Side Street Stop control  
NB Dellwood Dr approaching Woida Rd – one right/through, one left  
SB Dellwood Dr approaching Woida Rd – one right/through, one left  
WB Woida Rd approaching Dellwood Dr – one right/through/left  
EB Woida Rd approaching Dellwood Dr – one right, one through/left

TH 371 at Woida Road – Traffic Signal control  
NB 371 approaching Woida Rd – one right, two through, one left  
SB 371 approaching Woida Rd – one right, two through, two left  
WB Woida Rd approaching TH 371 – one free right, one through, one left  
EB Woida Rd approaching TH 371 – one free right, one through, one left

The existing roadway geometrics including the intersection traffic control, is shown on **Figure 3** and **Figure 4**.

**C. Traffic Volumes**

AM and PM peak hour turning movements and daily counts were conducted during the week of October 3<sup>rd</sup>, 2016. These counts were used as the existing baseline conditions for the area. **Figure 3** and **Figure 4** shows the existing intersections that were analyzed as part of this, with the existing 2016 AM and PM peak hour and traffic volumes.

**D. Crash Data**

The crash data included with this study was obtained using the Minnesota Crash Mapping Analysis Tool (MnCMAT) developed by MnDOT. The database includes crashes reported to MnDOT by local law enforcement agencies.

The crash data presented are for the years of 2013-2015. The MnCMAT database does not provide access to the original handwritten crash reports which contain some details that are not represented in the MnCMAT database. Crashes that resulted in damages under \$1000 may not be included in the database results as well.

The results indicate that only the intersection of Woida Road at Dellwood Drive and TH 371 at Woida Road had reported crashes. In both cases the calculated accident and severity rates are above the MnDOT Metro and Statewide averages. In general, the types of crashes include:

- Rear end crashes of vehicles stopped or slowing down to make left or right turns.
- Right angle crashes of vehicles pulling out from a side street or driveway.
- Left turn crashes of vehicles turning in front of another vehicle.

A summary of the existing crash data is shown in **Table 1**.

**Table 1 - Crash Summary**

Intersection	2013		2014		2015		Total Crashes
	PD	PI	PD	PI	PD	PI	
Dellwood Dr at Woida Rd	1	0	1	1	0	2	5
TH 371 at Woida Rd	18	5	12	4	21	10	70



Figure 3  
Existing Conditions  
Turning Movements

Northwoods Crossing Development  
City of Baxter, MN



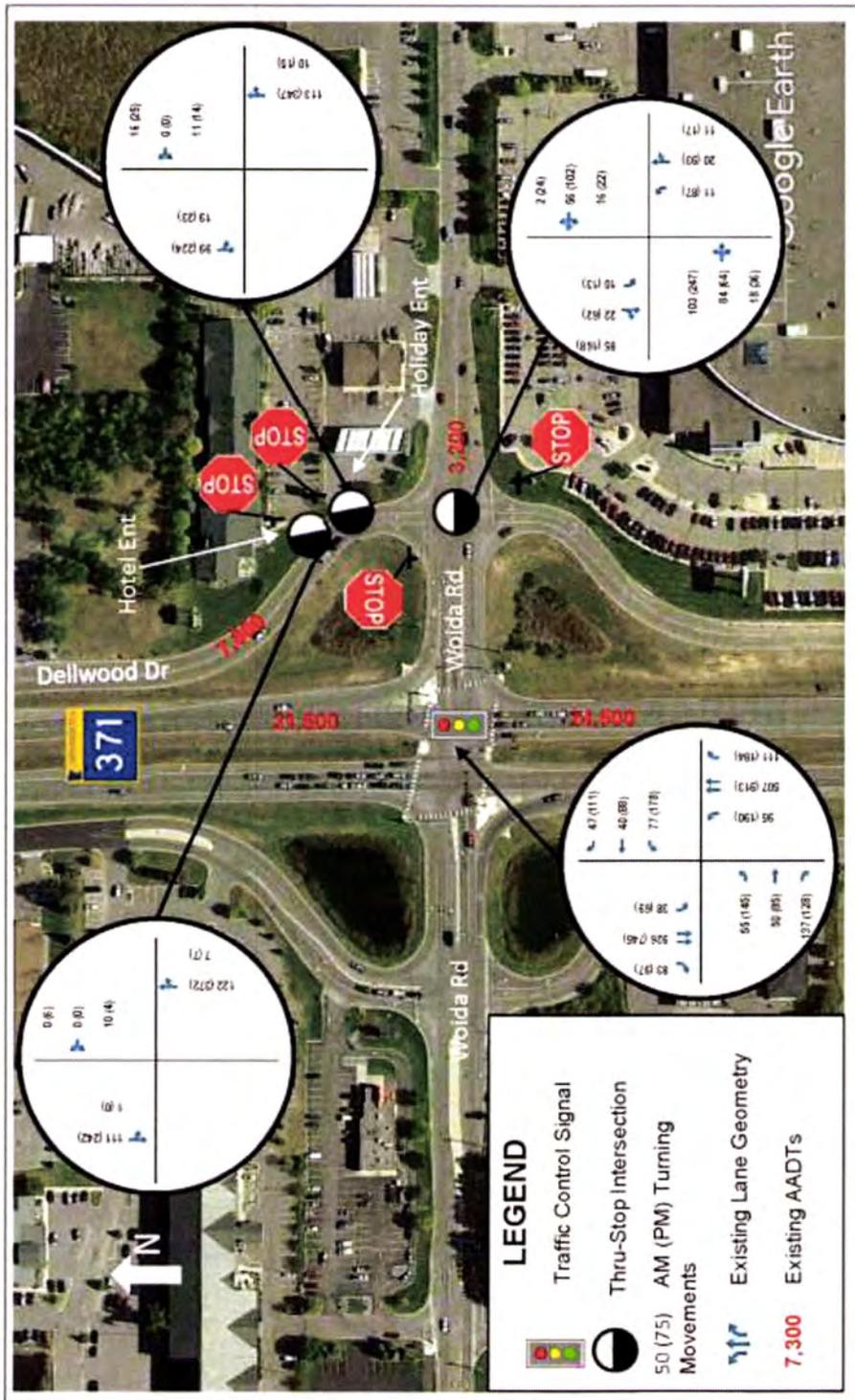


Figure 4  
Existing Conditions  
Turning Movements

Northwoods Crossing Development  
City of Baxter, MN



## TRAFFIC PROJECTIONS

In order to analyze the lane configuration and traffic control needs, projected traffic volumes were determined for the area. Projected 2018 and 2030 traffic volumes were determined based on proposed anticipated future development land use in the area and the City's current Transportation Plan. The following sections outline the traffic generation from the study area, as well as the traffic distribution and projected traffic volumes.

### A. Traffic Generation

Traffic growth in the vicinity of a proposed site will occur between existing conditions and any given future year due to other development within the region. This background growth must be accounted for and included in future year traffic forecasts. Reviewing the historical traffic counts in the area, traffic has stayed somewhat constant or dropped in the past few years. However, in order to account for some background growth in traffic the Crow Wing County State Aid traffic growth projection factor of 1.7 over a 20 year period was used to project traffic from the 2016 counts to the 2018 and 2030 analysis years.

The estimated trip generation from the proposed Northwoods Crossing Development project is shown in **Table 2**. The trip generation used to estimate the proposed site traffic is also based on rates for other similar land uses as documented in the Institute of Transportation Engineers *Trip Generation Manual*, 9<sup>th</sup> Edition. The table shows the weekday AM and PM peak hour trip generation for the proposed development.

**Table 2 - Estimated Dellwood Drive Development Site Trip Generation**

Planned Use	Size	Unit	AM Peak Hour			PM Peak Hour		
			Total	In	Out	Total	In	Out
Retail (2 sites)	2,000 ea	SF	28	14	14	20	12	8
Restaurant without drive through (2 sites)	40 ea	Seats	176	106	70	106	54	52
Restaurant with drive through	25	Seats	107	55	52	95	49	46
Coffee Shop with drive through	20	Seats	203	99	104	73	37	36
<b>Total New Trips</b>			<b>514</b>	<b>274</b>	<b>240</b>	<b>294</b>	<b>152</b>	<b>142</b>

Source: Institute of Transportation Engineers *Trip Generation Manual*, 9th Edition

**B. Traffic Distribution**

Area generated trips were distributed to the adjacent roadway system based on several factors including:

- Previous traffic and transportation studies in the area.
- Anticipated origins and destinations for specific land use (i.e. location of commercial uses in relationship to residential).
- Existing travel patterns and future roadway connections.

Based on these parameters the following general traffic distribution was used to distribute the projected traffic volumes along in the Dellwood Drive area:

- 10% to/from the north on Dellwood Drive
- 20% to/from north on TH 371
- 15% to/from south on Dellwood Drive
- 35% to/from south on TH 371
- 10% to/from the east on Woida Road
- 10% to/from the west on Woida Road

**C. Projected Traffic Volumes**

Traffic forecasts were prepared for the year 2018 which is the year after the initial phase the proposed development is anticipated to be completed and for the 2030 conditions the Comprehensive Plan year which represents the full development of the area.

The traffic forecasts were prepared by adding the projected annual background traffic growth and the projected non-development traffic growth to the existing 2016 traffic counts to determine the “No-Build” traffic conditions. The proposed development traffic was then added to the no-build traffic conditions to determine the 2018 and 2030 Build traffic conditions. **Figures 5 - 12** shows the projected 2018 and 2030 No-Build and Build AM and PM peak hour traffic volumes.

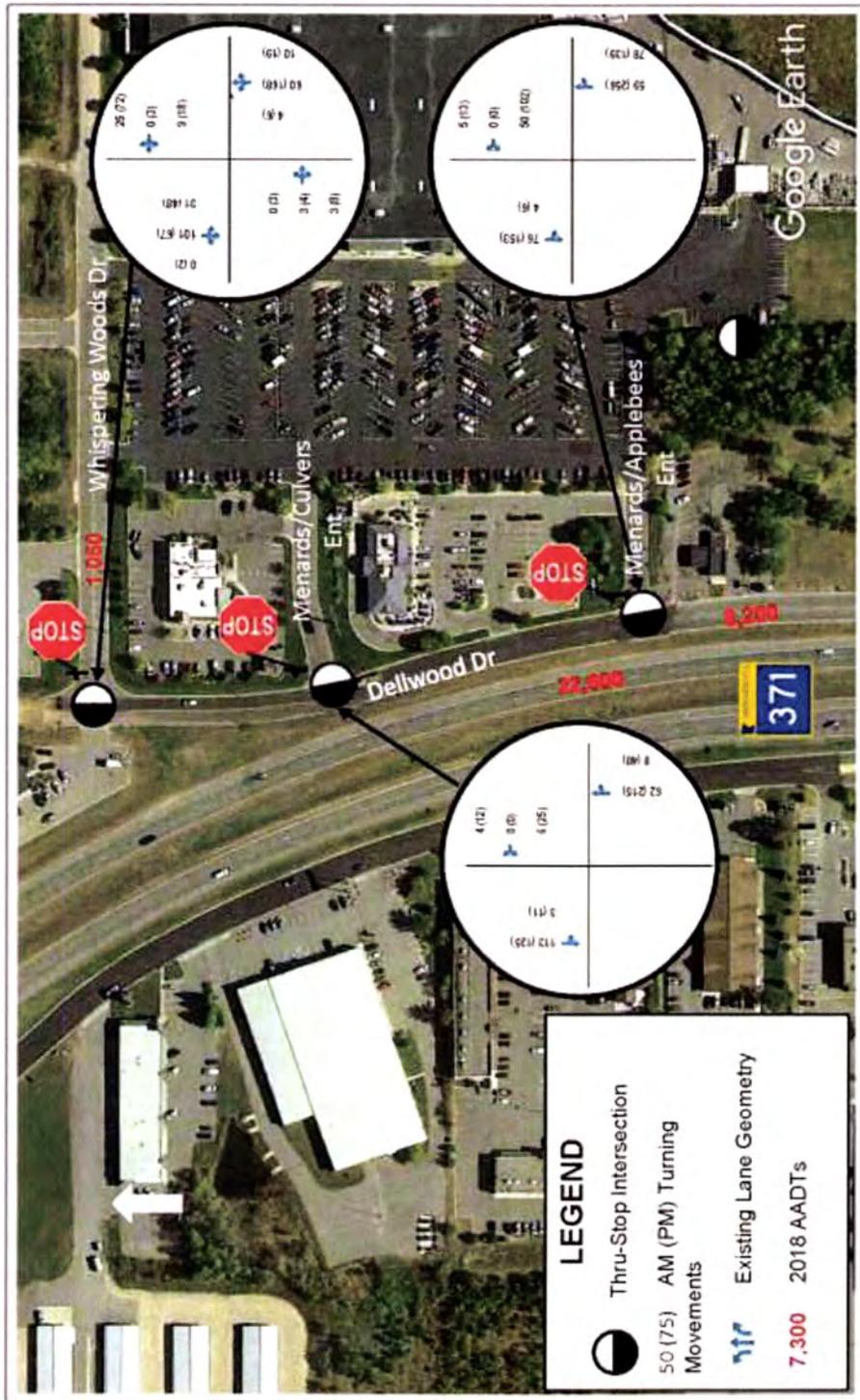


Figure 5  
2018 No-Build Conditions  
Turning Movements

Northwoods Crossing Development  
City of Baxter, MN



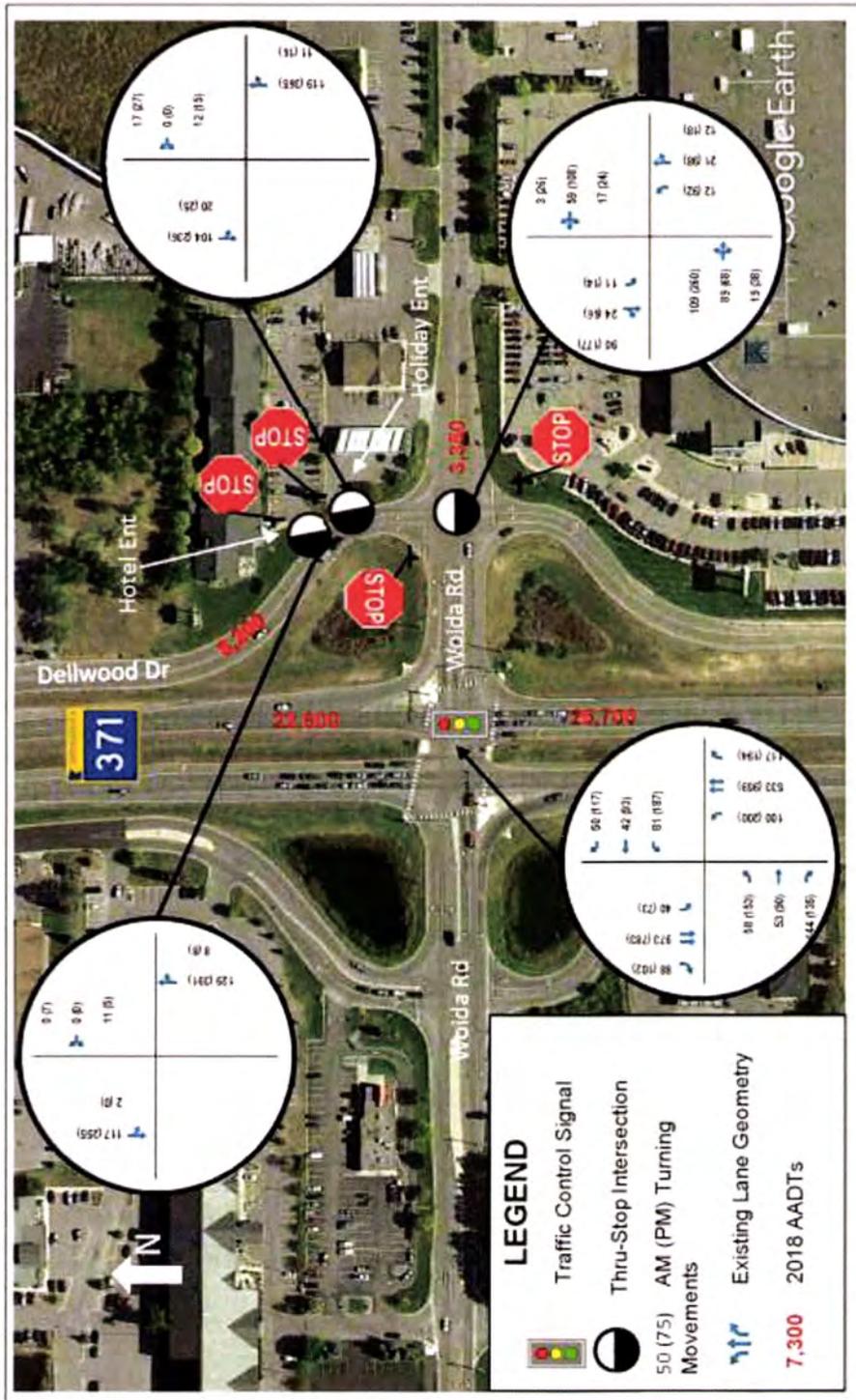


Figure 6  
2018 No-Build Conditions  
Turning Movements

Northwoods Crossing Development  
City of Baxter, MN





Figure 7  
2030 No-Build Conditions  
Turning Movements

Northwoods Crossing Development  
City of Baxter, MN

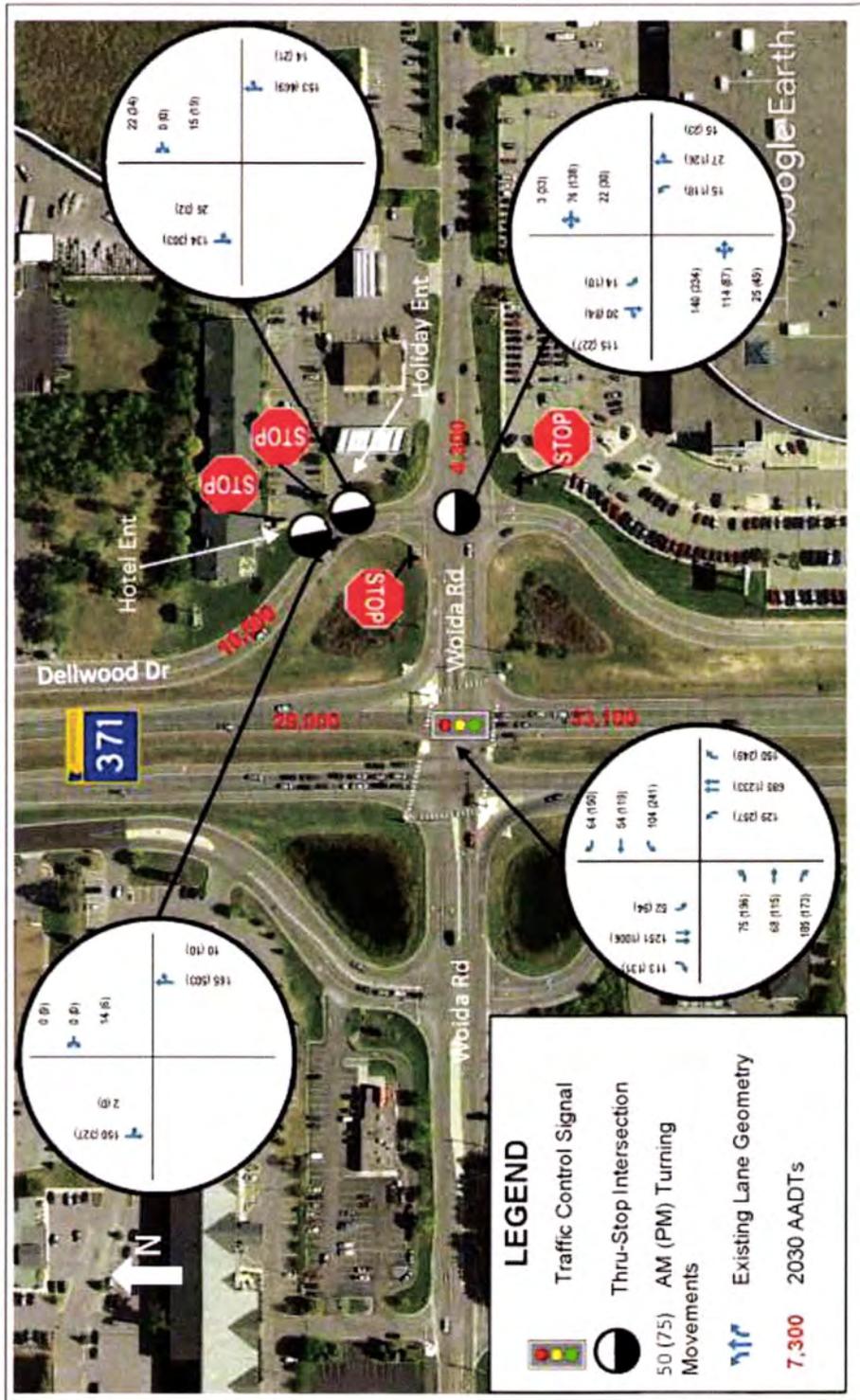


Figure 8  
2030 No-Build Conditions  
Turning Movements

Northwoods Crossing Development  
City of Baxter, MN



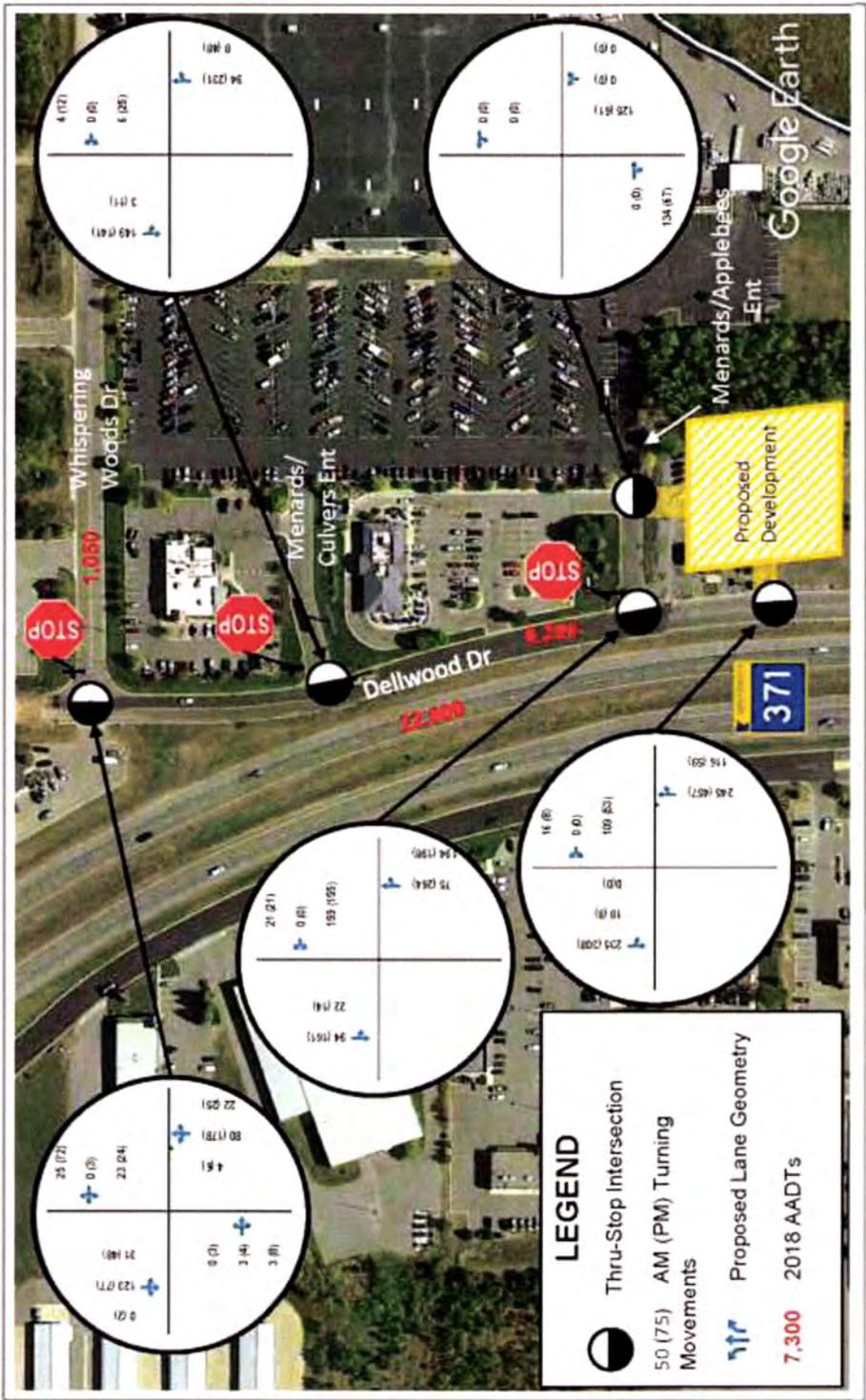


Figure 9  
2018 Build Conditions  
Turning Movements

Northwoods Crossing Development  
City of Baxter, MN



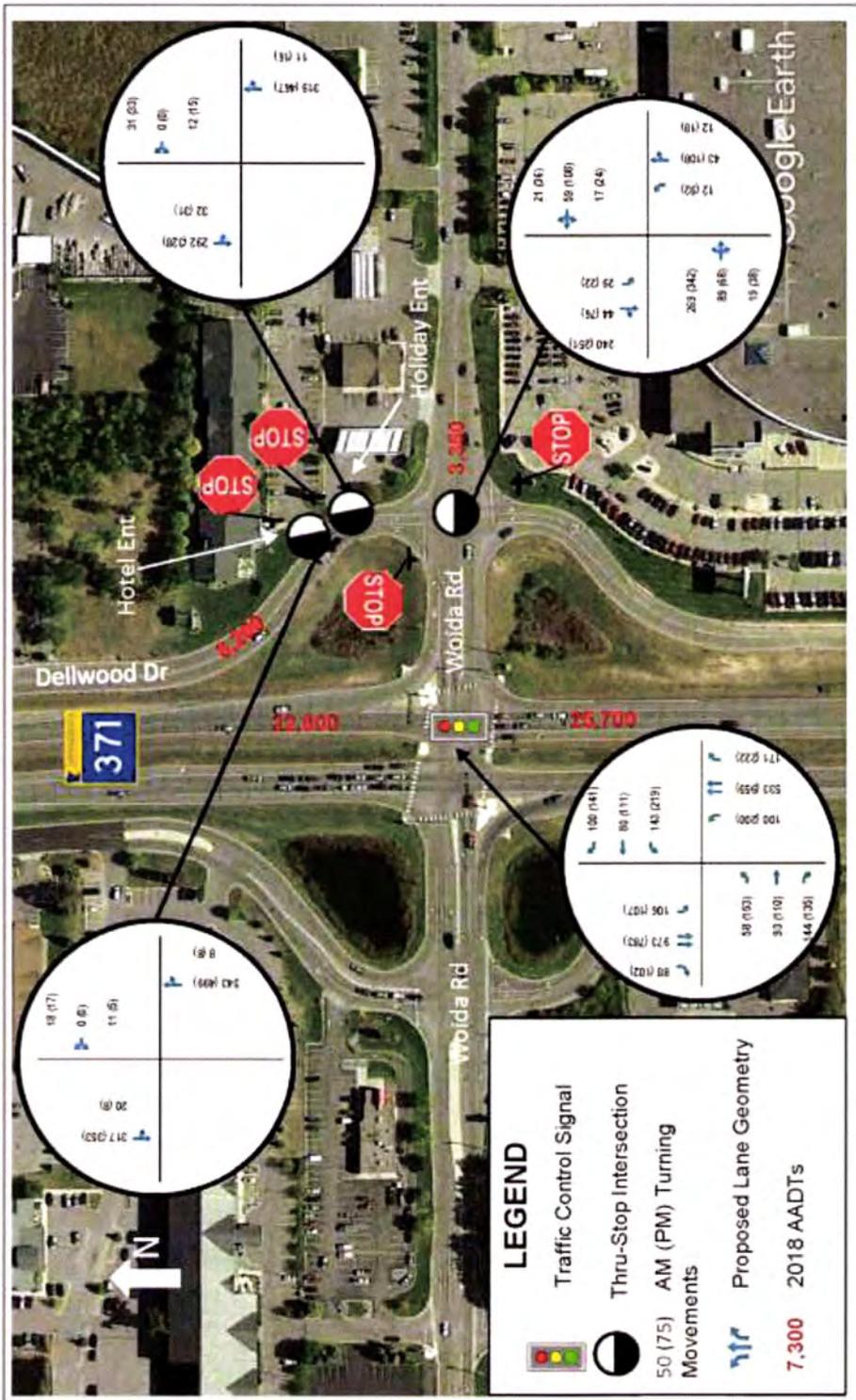


Figure 10  
2018 Build Conditions  
Turning Movements

Northwoods Crossing Development  
City of Baxter, MN



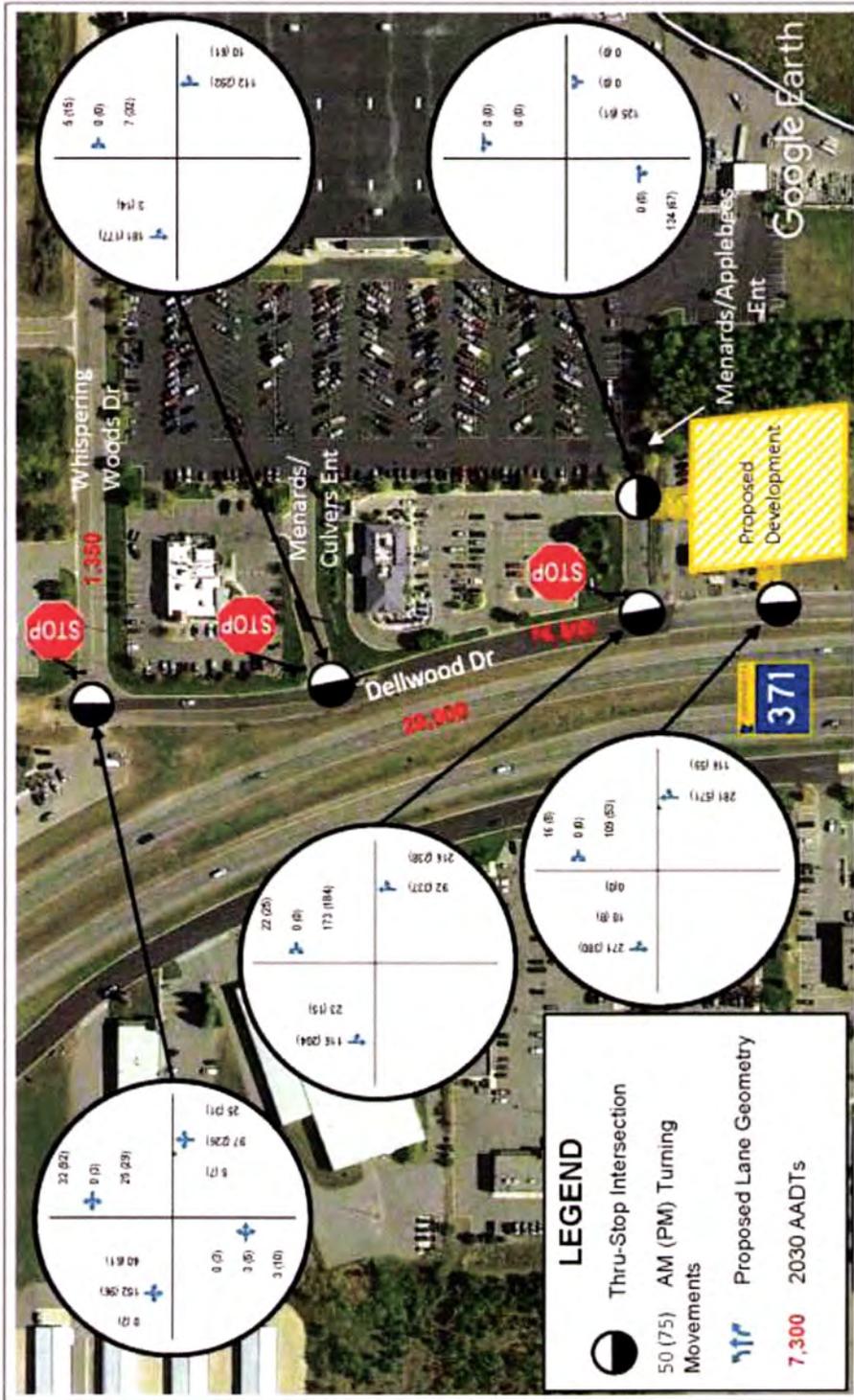
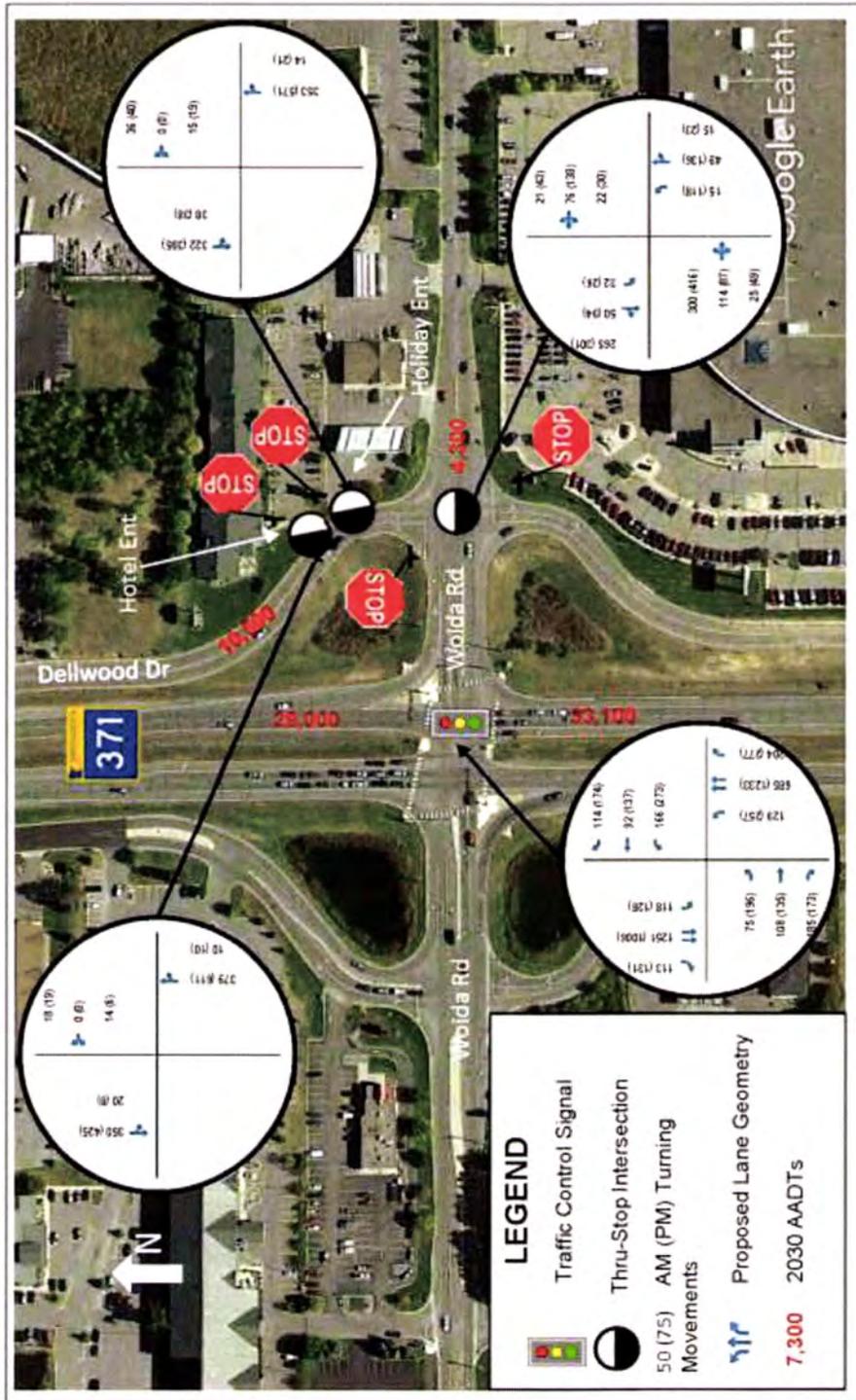


Figure 11  
2030 Build Conditions  
Turning Movements

Northwoods Crossing Development  
City of Baxter, MN





**Figure 12**  
**2030 Build Conditions**  
**Turning Movements**

**Northwoods Crossing Development**  
**City of Baxter, MN**

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## TRAFFIC IMPACT ANALYSIS

Existing and/or forecasted traffic operations were evaluated for the intersections and driveway in the study area. The analysis was conducted for the following scenarios:

1. Existing 2016
2. Projected 2018 No-Build
3. Projected 2018 Build
4. Projected 2030 No-Build
5. Projected 2030 Build

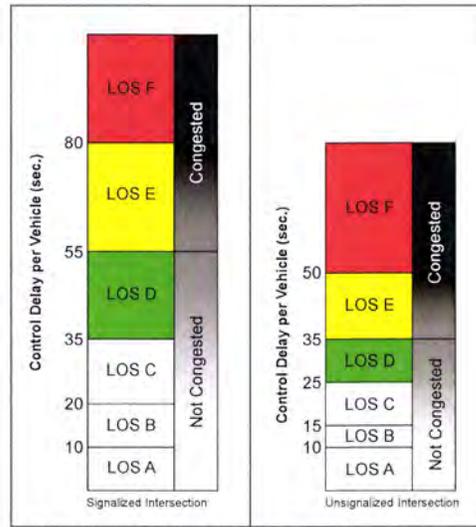
This section describes the methodology used to assess the operations and provides a summary of traffic operations for each scenario.

### **A. Methodology**

The intersections in the corridor were evaluated during the AM and PM peak hours using Synchro/SimTraffic micro simulation software. The results are derived from established methodologies documented in the Highway Capacity Manual (HCM) 2010. The software was used to evaluate the characteristics of the roadway network including lane geometrics, turning movement volumes, traffic control and signal timing. In addition, the signal timing parameters for future year conditions were optimized using Synchro. This information was then transferred to SimTraffic (the traffic simulation model) to estimate average peak hour vehicle delays and queues.

One of the primary measures of effectiveness used to evaluate intersection traffic operations, as defined in the HCM, is Level of Service (LOS) – a qualitative letter grade, A – F, based on seconds of vehicle delay due to a traffic control device at an intersection. By definition, LOS A conditions represent high quality operations (i.e., motorists experience very little delay or interference) and LOS F conditions represent very poor operations (i.e., extreme delay or severe congestion). **Figure 13** depicts a graphical interpretation of delay times that define level of service. The delay thresholds are lower for un-signalized intersections than signalized intersections due to the public's perception of acceptable delays for different traffic controls as indicated in the HCM. In accordance with the Minnesota Department of Transportation (MnDOT) guidelines, this analysis used the LOS D/E boundary as an indicator of acceptable traffic operations.

**Figure 13: Level of Service Ranges for Signalized and Un-signalized Intersections**



SOURCE: Level of Service thresholds from the Highway Capacity Manual, 2000

Existing Level of Service Summary

**Table 3 - 2016 Existing Conditions** shown below, summarizes the existing LOS at the primary intersections and driveways in the study area assuming the current lane geometry, traffic control and 2016 traffic volumes. The analysis results show that all intersection and driveways are operating at an overall LOS C or better during both the weekday AM and PM peak hours with all movements operating at LOS D or better, except the northbound left turn from TH 371 to westbound Woida Road in the PM Peak Hour which is operating at a LOS E.

**Table 3: 2016 Existing Conditions Level of Service Summary**

Intersection	AM Peak Hour		PM Peak Hour	
	LOS	Overall Delay (sec/veh)	LOS	Overall Delay (sec/veh)
Dellwood Dr at Whispering Woods Ln	A (A)	1	A (A)	1
Dellwood Dr at Menards N Access	A (A)	1	A (A)	1
Dellwood Dr at Menards/Culvers Access	A (A)	1	A (A)	2
Dellwood Dr at Hotel Access	A (A)	1	A (A)	1
Dellwood Dr at Holiday Access	A (A)	1	A (A)	1
Dellwood Dr at Woida Rd	A (A)	3	A (B)	5
TH 371 at Woida Rd	B (D)	15	C (E)	31

A (A) – Overall LOS (Worst Movement LOS)

Forecasted Traffic Operations

A capacity and LOS analysis was completed for the study area intersections and driveways for 2018 which is the year after the proposed initial development would be completed and for the 2030 condition which represents the 20 year design timeframe assumed to be full development of the area. The results of the analysis are discussed below.

**Table 4 – No Build Level of Service Summary**, shows that all intersection and driveways will be operating at an overall LOS D or better in 2018 and 2030 during both the weekday AM and PM peak hours with the existing intersection geometrics and traffic control, except TH 371 at Woida Road in the PM Peak Hour by the year 2030. With the increase in traffic, some movements will be operating at LOS E and F, specifically at the intersections of TH 371 and Woida Road.

The specific movements at LOS E or F are:

- 2018:
  - NB left turn from TH 371 to WB Woida Road, AM = LOS D, PM = LOS E
  - SB left turn from TH 371 to EB Woida Road, PM = LOS E
- 2030
  - NB left turn from TH 371 to WB Woida Road, AM = LOS E, PM = LOS F
  - SB left turn from TH 371 to EB Woida Road, AM = LOS E, PM = LOS F

**Table 4: No Build Condition Level of Service Summary**

Intersection	2018				2030			
	AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour	
	LOS	Delay (sec/veh)						
Dellwood Dr at Whispering Woods Ln	A (A)	1						
Dellwood Dr at Menards N Access	A (A)	1						
Dellwood Dr at Menards/Culvers Access	A (A)	1	A (A)	2	A (A)	1	A (C)	6
Dellwood Dr at Hotel Access	A (A)	1	A (B)	1	A (A)	1	B (C)	10
Dellwood Dr at Holiday Access	A (A)	1	A (C)	3	A (A)	1	B (C)	12
Dellwood Dr at Woida Rd	A (A)	3	A (B)	6	A (A)	3	B (C)	14
TH 371 at Woida Rd	B (D)	17	D (E)	44	D (E)	48	E (F)	56

A (A) – Overall LOS (Worst Movement LOS)

**Table 5 - Projected Build Level of Service Summary**, shows that, similar to the no-build conditions, all intersection and driveways will be operating at an overall LOS D or better in 2018 and 2030 during both the weekday AM and PM peak hours with the existing intersection geometrics and traffic control, except TH 371 at Woida Road in the PM Peak Hour by the year 2030. With the increase in traffic, some movements will be operating at LOS E and F, specifically at the intersections of TH 371 and Woida Road and Woida Road at Dellwood Drive.

The specific movements at LOS D, E or F are:

- 2018:
  - NB left turn from TH 371 to WB Woida Road, AM = LOS D, PM = LOS E
  - SB left turn from TH 371 to EB Woida Road, PM = LOS E
- 2030
  - WB left turn from Menards No Access to Dellwood Dr, PM = LOS D
  - WB left turn from Hotel Access to Dellwood Dr, PM = LOS D
  - WB left turn from Holiday Gas Access to Dellwood Dr, PM = LOS D
  - EB left turn from Woida Rd to Dellwood Dr, PM = LOS E
  - NB left turn from TH 371 to WB Woida Road, AM = LOS E, PM = LOS F
  - SB left turn from TH 371 to EB Woida Road, AM = LOS E, PM = LOS F

**Table 5: Build Condition Level of Service Summary**

Intersection	2018				2030			
	AM Peak Hour		PM Peak Hour		AM Peak Hour		PM Peak Hour	
	LOS	Delay (sec/veh)						
Dellwood Dr at Whispering Woods Ln	A (A)	1	A (A)	1	A (A)	1	A (A)	2
Dellwood Dr at Menards N Access	A (A)	1	A (A)	1	A (A)	1	A (B)	3
Dellwood Dr at Menards/Culvers Access	A (A)	3	A (A)	3	A (A)	3	C (D)	18
Dellwood Dr at Site Access	A (A)	1	A (B)	5	A (B)	2	B (C)	12
Dellwood Dr at Hotel Access	A (A)	2	A (B)	5	A (A)	3	C (D)	20
Dellwood Dr at Holiday Access	A (A)	2	A (C)	8	A (C)	4	C (D)	18
Dellwood Dr at Woida Rd	A (B)	4	B (C)	10	A (B)	6	D (E)	26
TH 371 at Woida Rd	B (D)	18	D (E)	46	D (E)	49	E (F)	59

A (A) – Overall LOS (Worst Movement LOS)

### Vehicle Queuing Analysis

A queuing analysis for the existing and future conditions was conducted, evaluating the anticipated vehicle queues with the build traffic conditions. The analysis was conducted using the SimTraffic simulation software. The results found that during both the weekday AM and PM peak hours in 2018 the maximum queues will not exceed the available turn lane storage or block driveways. However, by 2030 several locations were identified that would exceed turn lane storage and would block driveways. These include the following locations:

- Northbound and southbound TH 371 left turn lanes to Woida Road will back up past existing left turn storage length.
- Eastbound Woida Rd left turn lane to northbound Dellwood Drive will back up to TH 371.
- Southbound Dellwood Drive through/right lane to Woida Road will back up past the Holiday gas access and Hotel access.
- Southbound Dellwood Drive left turn lane to Holiday gas access will back up past the Hotel access.
- Westbound movement exiting Menards south access/Culvers access will back up to the Culvers/proposed Northwoods Crossing Development driveway.
- Westbound movement exiting the Hotel access will back up blocking site parking.
- Westbound movement exiting the proposed Northwoods Crossing Development site will back up blocking site parking.

## **SITE PLAN REVIEW**

Based on a review of the proposed Northwoods Crossing Development site plan as shown in *Figure 2* the following comments are offered:

1. The drive through isles should have a separation from the site circulation drive isles. This should be some type of physical barrier, such as a raised concrete median.
2. The configuration of the access driveway from the Menards/Culvers Restaurant driveway should be modified to provide a clear lane configuration. This could be accomplished by having the driveway line up directly with circulation drive isle in front of the building and “Tee” the back circulation isle into it.
3. At the locations where the drive through isles cross the circulation drive isles in the back of the building, clear delineation and signing should be provided so that the circulation isles are not blocked.
4. A truck circulation plan, including truck turning radius templates should be provided to insure that trucks can move through the site without impacting parking or drive isles.

## CONCLUSIONS / RECOMMENDATIONS

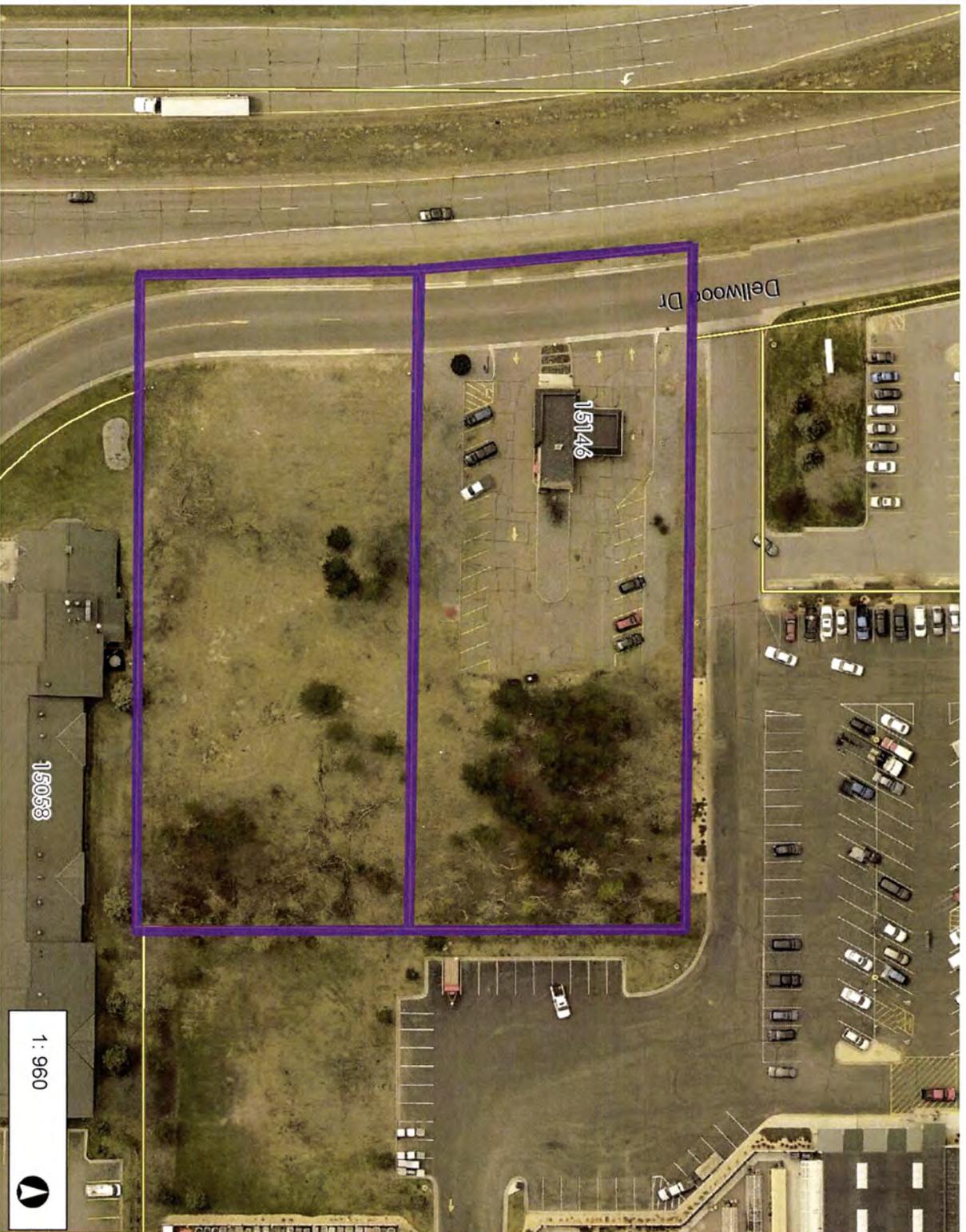
Based on the analysis documented in this Study, WSB has concluded the following:

- The proposed Northwoods Crossing Development is anticipated to consist of two 6000 sf buildings with a mixture of service retail and restaurant uses. All access from the site will be through driveway connections to Dellwood Drive; one full movement direct access to Dellwood Drive, and one shared driveway with Menards across from the existing Culvers driveway. The development is anticipated to generate an additional 514 AM peak hour trips and 294 PM peak hour trips.
- Existing traffic operations analysis shows that all intersection and driveways are operating at an overall LOS C or better during both the weekday AM and PM peak hours with all movements operating at LOS D or better, except the northbound left turn from TH 371 to westbound Woida Road in the PM Peak Hour which is operating at a LOS E.
- Intersection traffic operations for the future no-build condition show that all intersection and driveways will be operating at an overall LOS D or better in 2018 and 2030 during both the weekday AM and PM peak hours with the existing intersection geometrics and traffic control, except TH 371 at Woida Road in the PM Peak Hour by the year 2030. With the increase in traffic, some movements will be operating at LOS E and F, specifically at the intersections of TH 371 and Woida Road.
- Intersection traffic operations for the future build condition show that, assuming the addition of the Northwoods Crossing Development traffic, all intersection and driveways will be operating at an overall LOS D or better in 2018 and 2030 during both the weekday AM and PM peak hours with the existing intersection geometrics and traffic control, except TH 371 at Woida Road in the PM Peak Hour by the year 2030. With the increase in traffic, some movements will be operating at LOS E and F, specifically at the intersections of TH 371 and Woida Road and Woida Road at Dellwood Drive.
- Based on the vehicle queuing analysis for the existing and future traffic conditions for weekday AM and PM peak hours in 2018 the maximum queues will not exceed the available turn lane storage or block driveways. However, by 2030 several locations were identified that would exceed turn lane storage and would block driveways.

Based on these conclusions, the traffic analysis and review of the site plan the following is recommended:

1. Restripe Dellwood Drive to provide a center left turn lane from Woida Road to north of the Menards south access/Culvers access with the proposed Northwoods Crossing Development plan.
2. As traffic continues to increase with future development, review the operations and roadway geometric needs at the Woida Road and Dellwood Drive intersection.
3. As traffic continues to increase with future development, work with MnDOT to extend the left turn lanes on TH 371 at Woida Road intersection.

# Northwoods Crossing



160.0  
 0 80.00 160.0 Feet  
 NAD\_1983\_HARN\_Adl\_MN\_Crow\_Wing\_Feet  
 City of Baxter

This map is a user generated static output from an Internet mapping site and is for reference only. Data layers that appear on this map may or may not be accurate, current, or otherwise reliable.  
 THIS MAP IS NOT TO BE USED FOR NAVIGATION

1 : 960



**Legend**

- Streets
- Locations
  - Current
  - Pending
  - Vacant
  - Retired
  - Other
- Parcels
- Land Use Cases
- Encumbrances
- Municipal Boundaries
- Historical Parcels
- 2016 Imagery
  - Red: Band\_1
  - Green: Band\_2
  - Blue: Band\_3

Notes

# NORTHWOODS CROSSING CITY OF BAXTER, MINNESOTA CROW WING COUNTY COUNTY, MINNESOTA



## GOVERNING SPECIFICATIONS

THE 2016 EDITION OF THE MINNESOTA DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR CONSTRUCTION" AND THE 2016 EDITION OF THE "MATERIALS LAB SUPPLEMENTAL SPECIFICATIONS FOR CONSTRUCTION" SHALL GOVERN.

Sheet Number	Sheet Title
C1.0	TITLE SHEET
C2.0	LEGEND
C3.0	EXISTING CONDITIONS AND REMOVALS
C5.0	SITE PLAN
C5.1	TURNING MOVEMENTS
C6.0	UTILITY PLAN
C7.0	GRADING PLAN
C8.0	ADA STANDARD PLAN
C8.1	ADA STANDARD PLAN
C8.2	ADA STANDARD PLAN
C8.3	ADA STANDARD PLAN
C8.4	ADA STANDARD PLAN
C9.0	CONSTRUCTION DETAIL
C9.1	CONSTRUCTION DETAIL
C9.2	CONSTRUCTION DETAIL

PRELIMINARY - NOT FOR CONSTRUCTION



**SURVEY MONUMENTS**

- ◆ BENCH MARK
- FOUND CIM
- ▲ FOUND CPNT.
- FOUND JLM
- FOUND LATH
- FOUND PIPE
- FOUND READING
- STAKED CIM
- ▲ STAKED CPNT.
- STAKED JLM
- STAKED PIPE

**EXISTING TOPO SYMBOLS**

- AC UNIT
- FENCE POST
- ▲ FLAG POLE
- GUARD POST
- GUY ANCHOR
- GUY POLE
- ♿ HANDICAP SYMBOL
- ✕ MAILBOX
- SHRUB
- SIGN DOUBLE POST
- SIGN SINGLE POST
- TREE CONIFER
- TREE DECIDUOUS
- ▲ TREE STUMP
- TV DISH
- WETLAND SYMBOL
- YARD LIGHT

**EXISTING UTILITY MUNICIPAL SYMBOLS**

- APRON
- Ⓢ LIFT STATION
- SANITARY CLEANOUT
- SANITARY MANHOLE
- STORM CATCH BASIN
- STORM INLET

**EXISTING UTILITY MUNICIPAL SYMBOLS (cont.)**

- ⊙ STORM MANHOLE
- ⊙ WATER CURB STOP
- ⊙ WATER HANDHOLE
- ⊙ WATER HYDRANT
- ⊙ WATER MANHOLE
- ⊙ WATER METER
- ⊙ WATER VALVE
- ⊙ WATER WELL
- UTILITY UTILITY SIZE & TYPE

**EXISTING UTILITY PRIVATE SYMBOLS**

- ⚡ ELEC GROUND LIGHT
- ⊙ ELEC HANDHOLE
- ⊙ ELEC LIGHT POLE
- ⊙ ELEC MANHOLE
- ⊙ ELEC METER
- ⊙ ELEC PEDESTAL
- ⊙ ELEC POLE
- ⊙ ELEC SIGNAL
- ⊙ ELEC TRANSFORMER BOX
- ⊙ GAS METER
- ⊙ GAS VALVE
- ⊙ LP TANK
- ⊙ TELE HANDHOLE
- ⊙ TELE MANHOLE
- ⊙ TELE PEDESTAL
- ⊙ TELE POLE
- ⊙ TV HANDHOLE
- ⊙ TV PEDESTAL

**PROPOSED UTILITY PRIVATE SYMBOLS**

- ⚡ ELEC LIGHT POLE PROPOSED
- ⊙ SURFACE DRAINAGE ARROW
- ⊙ STORM DRAIN INLET PROTECTION

**SOIL BORING SYMBOLS**

- LF LASER-INDUCED FLUORESCENCE BORING
- LY LYSIMETER
- MW MONITOR WELL
- PT PERC TEST
- PZ PIEZOMETER
- RW RECOVERY WELL
- SB SOIL BORING
- VP SOIL VAPOR POINT
- VV VAPOR SURVEY POINT

**PROPOSED UTILITY MUNICIPAL SYMBOLS**

- ▲ APRON PROPOSED
- SANITARY CLEANOUT PROPOSED
- SANITARY LIFT STATION PROPOSED
- SANITARY LIFT STATION VALVE MANHOLE PROPOSED
- SANITARY MANHOLE PROPOSED
- SANITARY PLUG PROPOSED
- STORM CATCH BASIN PROPOSED
- STORM MANHOLE PROPOSED
- WATER 11 1/4" BEND PROPOSED
- WATER 22 1/2" BEND PROPOSED
- WATER 45° BEND PROPOSED
- WATER 90° BEND PROPOSED
- WATER CAP PROPOSED
- WATER CROSS PROPOSED
- WATER CURB STOP PROPOSED
- WATER HYDRANT PROPOSED
- WATER REDUCER PROPOSED
- WATER SLEEVE PROPOSED
- WATER TEE PROPOSED
- WATER VALVE PROPOSED

**EXISTING TOPOGRAPHIC LINES**

- CENTER LINE
- EDGE OF WOODS
- FENCE BARB WIRE
- FENCE CHAIN LINK
- FENCE WOOD
- FORCEMAIN
- OVERHEAD CABLE TV
- OVERHEAD ELECTRIC
- OVERHEAD TELE
- RAILROAD
- RETAINING WALL
- SANITARY SEWER
- SANITARY SEWER SERVICE
- STORM SEWER
- STORM SEWER DRAIN TILE
- UNDERGROUND CABLE TV
- UNDERGROUND ELECTRIC
- UNDERGROUND FIBER OPTIC
- UNDERGROUND GAS
- UNDERGROUND TELE
- WATERMAIN
- WATERMAIN SERVICE
- WETLAND EDGE

**RAW, LOT & EASEMENTS LINES**

- BUILDING SETBACK LINE
- LOT LINE PROPOSED
- EASEMENT LINE
- EASEMENT LINE PROPOSED
- LOT LINE
- MNDOT CONTROLLED ACCESS LINE
- RIGHT OF WAY EXISTING
- RIGHT OF WAY PROPOSED

**PROPOSED CONSTRUCTION LINES**

- FENCE CHAIN LINK PROPOSED
- FENCE WOOD PROPOSED
- FENCE BARB WIRE PROPOSED
- FORCEMAIN PROPOSED
- SANITARY SEWER PROPOSED
- SANITARY SERVICE PROPOSED
- STORM SEWER PROPOSED
- STORM SEWER DRAIN TILE PROPOSED
- WATERMAIN PROPOSED
- WATERMAIN SERVICE PROPOSED

**EROSION CONTROL LINES**

- BALE CHECK
- BIO ROLL
- SILT FENCE
- SILT FENCE TYPE HEAVY DUTY
- SILT FENCE TYPE MACHINE SLICED
- SILT FENCE TYPE PREASSEMBLED
- FLOTATION SILT CURTAIN

**HATCH PATTERN AND SHADING LEGEND**

- RANDOM RIPRAP
- SOD
- SEED
- HYDRAULIC STABILIZER
- EROSION CONTROL BLANKET
- TEMP. ROCK CONSTRUCTION ENTRANCE
- BUILDING WALL HATCH
- BITUMINOUS SURFACE
- CONCRETE SURFACE
- GRAVEL SURFACE
- EASEMENT PATTERN

**DOCUMENTATION SYMBOLS**

- SECTION ARROW -
- SECTION NUMBER TOP.
- PAGE OF SECTION BOTTOM

**PRELIMINARY - NOT FOR CONSTRUCTION**



DATE: MARCH 2018  
PROJECT: NORTHWOODS CROSSING  
SHEET NO. C20

NO.	REVISIONS DESCRIPTION	DATE	BY

DATE: NOVEMBER 2016  
SCALE: AS SHOWN  
DRAWN BY: MBR  
CHECKED BY: MBR  
JOB NUMBER: 0297B0055.000



DATE: NOVEMBER, 2016  
SCALE: AS SHOWN  
DRAWN BY: MER  
CHECKED BY: MER  
JOB NUMBER: 0287B0055.000

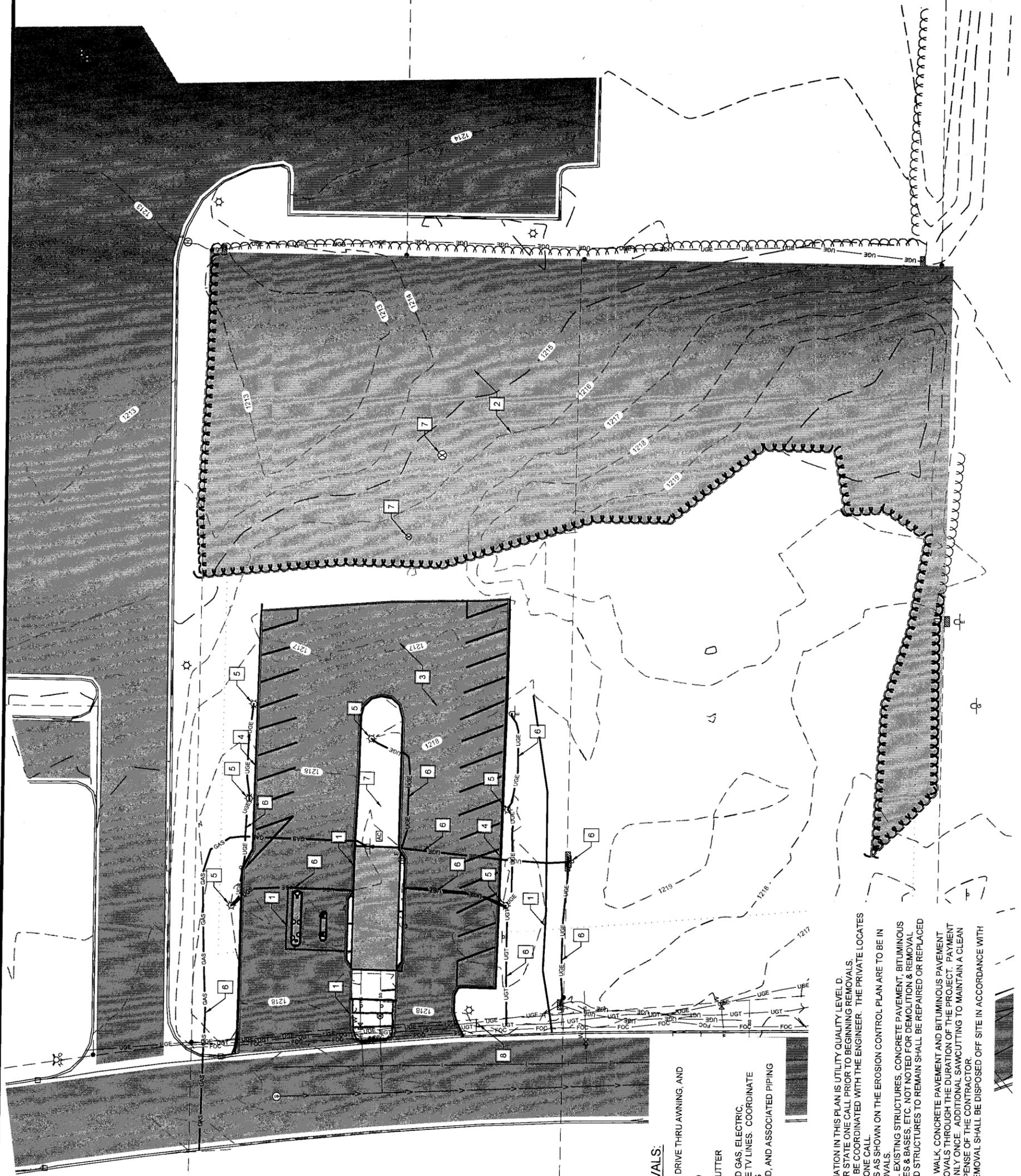
DATE	REV#	REVISIONS DESCRIPTION

DATE: NOVEMBER, 2016  
SCALE: AS SHOWN  
DRAWN BY: MER  
CHECKED BY: MER  
JOB NUMBER: 0287B0055.000

NORTHWOODS CROSSING  
BARATTO BROTHERS CONSTRUCTION  
BAXTER, MINNESOTA  
EXISTING CONDITIONS AND REMOVALS

SHEET NO. C3.0

# PRELIMINARY - NOT FOR CONSTRUCTION



### DEMOLITION AND REMOVALS:

1. REMOVE BUILDING, FOUNDATION, DRIVE THRU AWNING, AND LANDSCAPING
2. CLEAR AND GRUB (AREA SHADED)
3. REMOVE BITUMINOUS PAVEMENT
4. REMOVE CONCRETE CURB AND GUTTER
5. REMOVE LIGHT POLES
6. REMOVE EXISTING UNDERGROUND GAS, ELECTRIC, TELECOMMUNICATION, AND CABLE TV LINES. COORDINATE REMOVALS WITH UTILITY OWNERS
7. REMOVE SEPTIC TANK, DRAINFIELD, AND ASSOCIATED PIPING
8. SALVAGE SIGN

### NOTES:

- THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D.
- CONTRACTOR SHALL CALL GOPHER STATE ONE CALL PRIOR TO BEGINNING REMOVALS.
- PRIVATE UTILITY LOCATES ARE TO BE COORDINATED WITH THE ENGINEER. THE PRIVATE LOCATES WILL BE DONE BY GOPHER STATE ONE CALL.
- ALL EROSION CONTROL MEASURES AS SHOWN ON THE EROSION CONTROL PLAN ARE TO BE IN PLACE PRIOR TO BEGINNING REMOVALS.
- CONTRACTOR SHALL PROTECT ALL EXISTING STRUCTURES, CONCRETE PAVEMENT, BITUMINOUS PAVEMENT, BUILDINGS, LIGHT POLES & BASES, ETC. NOT NOTED FOR DEMOLITION & REMOVAL DURING REMOVALS. ANY DAMAGED STRUCTURES TO REMAIN SHALL BE REPAIRED OR REPLACED AT NO COST TO THE OWNER.
- ALL SAWCUT EDGES IN CONCRETE WALK, CONCRETE PAVEMENT AND BITUMINOUS PAVEMENT SHALL BE PROTECTED AFTER REMOVALS THROUGH THE DURATION OF THE PROJECT. PAYMENT FOR SAWCUTTING WILL BE MADE ONLY ONCE. ADDITIONAL SAWCUTTING TO MAINTAIN A CLEAN SAWCUT EDGE WILL BE AT THE EXPENSE OF THE CONTRACTOR.
- ALL MATERIALS IDENTIFIED FOR REMOVAL SHALL BE DISPOSED OFF SITE IN ACCORDANCE WITH STATE AND LOCAL LAWS.

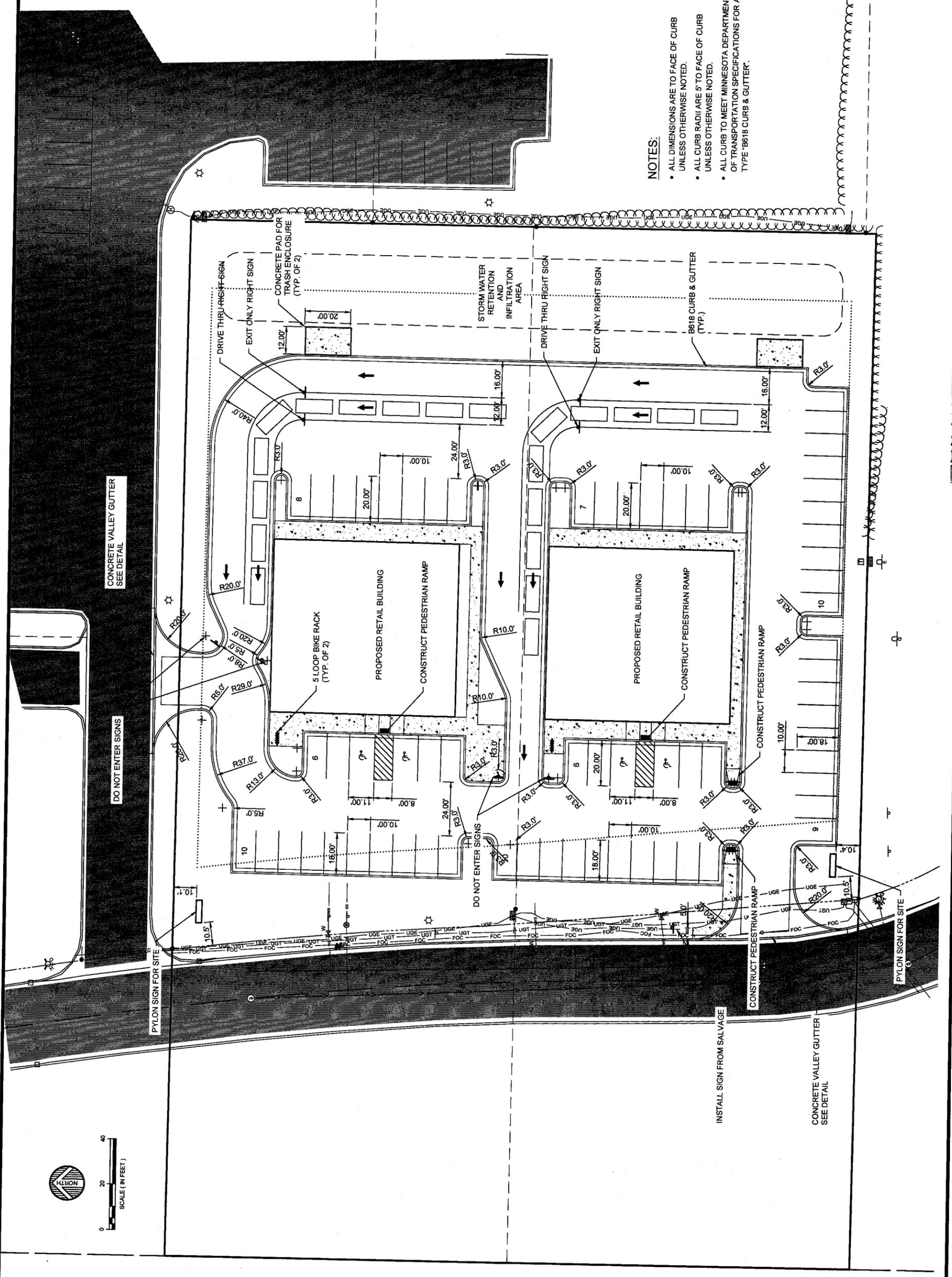
DATE	REV#	REVISIONS DESCRIPTION

BY: MICHAEL E. WIDE  
 DATE: MM/DD/YYYY, NO. 43996  
 I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.



# PRELIMINARY - NOT FOR CONSTRUCTION

- NOTES:**
- ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
  - ALL CURB RADII ARE 5' TO FACE OF CURB UNLESS OTHERWISE NOTED.
  - ALL CURB TO MEET MINNESOTA DEPARTMENT OF TRANSPORTATION SPECIFICATIONS FOR A TYPE 'B618 CURB & GUTTER'.



CONCRETE VALLEY GUTTER SEE DETAIL

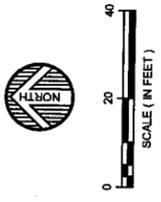
DO NOT ENTER SIGNS

PYLON SIGN FOR SITE

INSTALL SIGN FROM SALVAGE

CONCRETE VALLEY GUTTER SEE DETAIL

PYLON SIGN FOR SITE



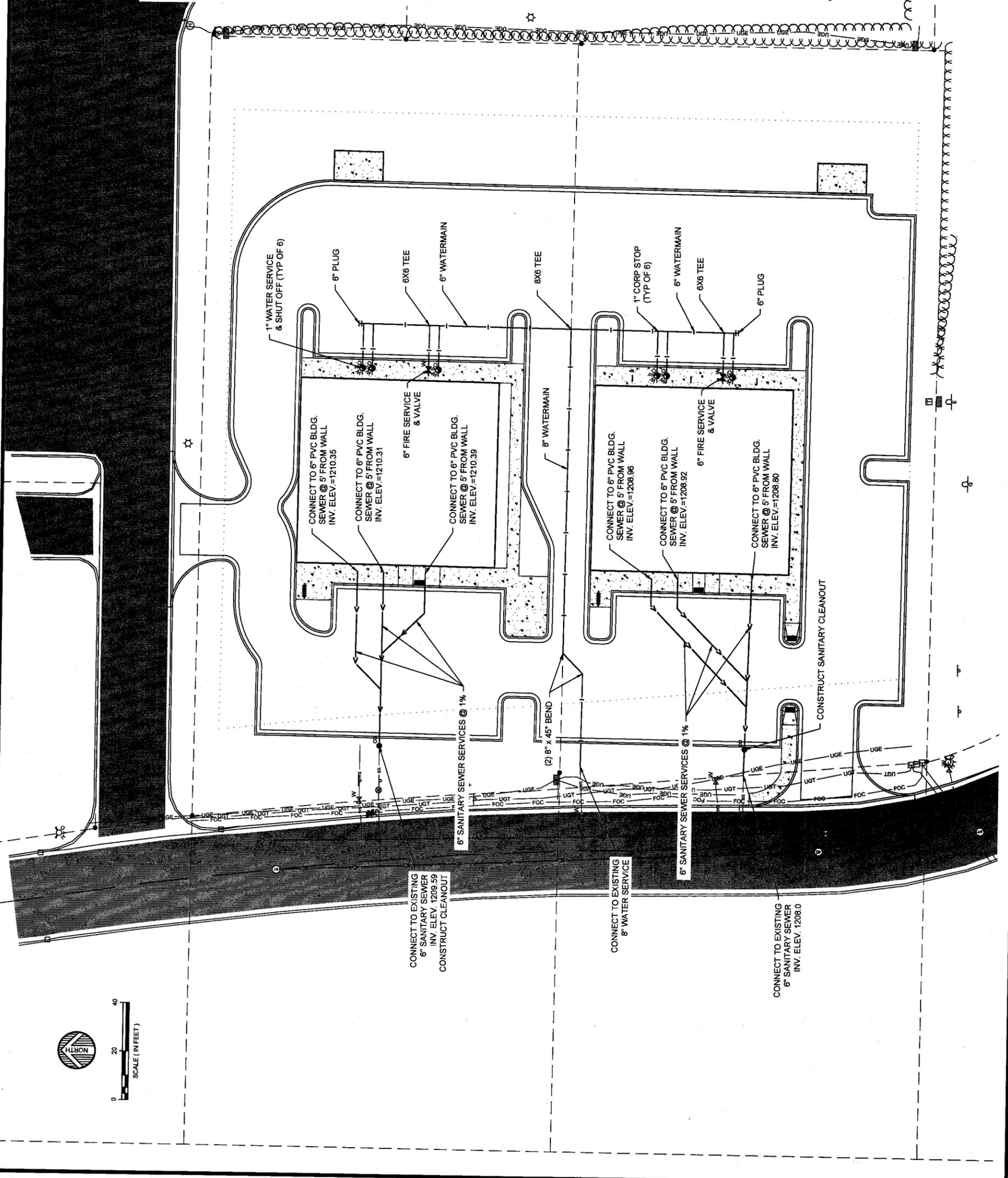


# PRELIMINARY - NOT FOR CONSTRUCTION

## NOTES:

- THE ENGINEER HAS MADE AN ATTEMPT TO SHOW ALL PUBLIC UTILITIES WITHIN THE CONSTRUCTION LIMITS OF THIS PROJECT. PUBLIC UTILITIES SHOWN ON THIS PLAN WERE DRAWN USING FIELD SURVEY INFORMATION AND MAPS PROVIDED TO THE ENGINEER BY THE UTILITY COMPANIES AS A RESULT OF A GOPHER STATE ONE CALL DESIGN LOCATE REQUEST. IT IS THE CONTRACTORS RESPONSIBILITY TO VERIFY THESE LOCATIONS PRIOR TO BIDDING AND CONSTRUCTING THE PROJECT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL EXCAVATION LOCATES AND SHALL NOTIFY ALL AFFECTED UTILITY COMPANIES AT LEAST 48-HOURS BEFORE CONSTRUCTION.
- THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D. THIS QUALITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-2, ENTITLED "STANDARD GUIDELINES FOR THE COLLECTION AND DEPICTION OF EXISTING SUBSURFACE UTILITY DATA".
- UNLESS OTHERWISE NOTED, ALL MATERIALS, CONSTRUCTION TECHNIQUES AND TESTING SHALL CONFORM TO THE 1999 EDITION (OR LATEST EDITION) OF THE "STANDARD SPECIFICATIONS FOR TRENCH EXCAVATION AND BACKFILL / SURFACE RESTORATION, WATER MAIN AND SERVICE LINE INSTALLATION AND SANITARY SEWER AND STORM SEWER INSTALLATION" AS PUBLISHED BY THE CITY ENGINEER'S ASSOCIATION OF MINNESOTA AND TO THE "STANDARD SPECIFICATIONS FOR CONSTRUCTION" AS PUBLISHED BY THE MINNESOTA DEPARTMENT OF TRANSPORTATION, 2005 EDITION. THE CONTRACTOR SHALL BE REQUIRED TO FOLLOW ALL PROCEDURES AS OUTLINED BY THE LOCAL AGENCY AND THE MINNESOTA PLUMBING CODE.
- THE CONTRACTOR SHALL RECEIVE THE NECESSARY PERMITS FOR ALL WORK OUTSIDE THE PROPERTY LIMITS.
- COORDINATE SERVICE LOCATION ENTRIES WITH THE MECHANICAL ENGINEER.
- WATER SERVICE LINES SHALL BE INSTALLED WITH A MINIMUM OF 8.5 FEET OF COVER.
- RIM ELEVATIONS REFLECT A SUMP OF 0.05 FEET.
- STORM SEWER OF DIFFERING DIAMETERS SHALL MATCH 0.8 DIAMETER GRADE LINE ELEVATIONS AT CATCH BASIN OR MANHOLE JUNCTIONS.
- PROVIDE WATER MAIN THRUST RESTRAINTS PER CITY STANDARD REQUIREMENTS.
- A MINIMUM OF 18 INCH VERTICAL SEPARATION SHALL BE REQUIRED AT ALL WATER MAIN CROSSINGS WITH SANITARY SEWER AND STORM SEWER.
- UTILITY SERVICES TERMINATE 5 FEET OUTSIDE THE BUILDING WALL UNLESS OTHERWISE STATED. SEE ARCHITECTURAL PLAN OR MECHANICAL PLAN FOR EXACT LOCATION AND CONSTRUCTION DETAILS FROM BUILDING WALL TO 5 FEET OUTSIDE BUILDING WALL.
- HDPE STORM SEWER PIPE SHALL MEET THE REQUIREMENTS OF AASHTO M284, TYPE S WITH WATER TIGHT JOINTS. PVC STORM SEWER SHALL BE SCHEDULE 40 PIPE. FLARED ENDS SHALL BE RCP WITH TRASH GUARDS AND RIP RAP.

## GENERAL UTILITY NOTES:



# PRELIMINARY - NOT FOR CONSTRUCTION

SHEET NO. **C9.0**

NORTHWOODS CROSSING  
BARTO BROTHERS CONSTRUCTION

CONSTRUCTION DETAIL

DATE: NOVEMBER, 2016  
SCALE: AS SHOWN  
DRAWN BY: INITIALS  
CHECKED BY: MER  
JOB NUMBER: 0287B0055.000

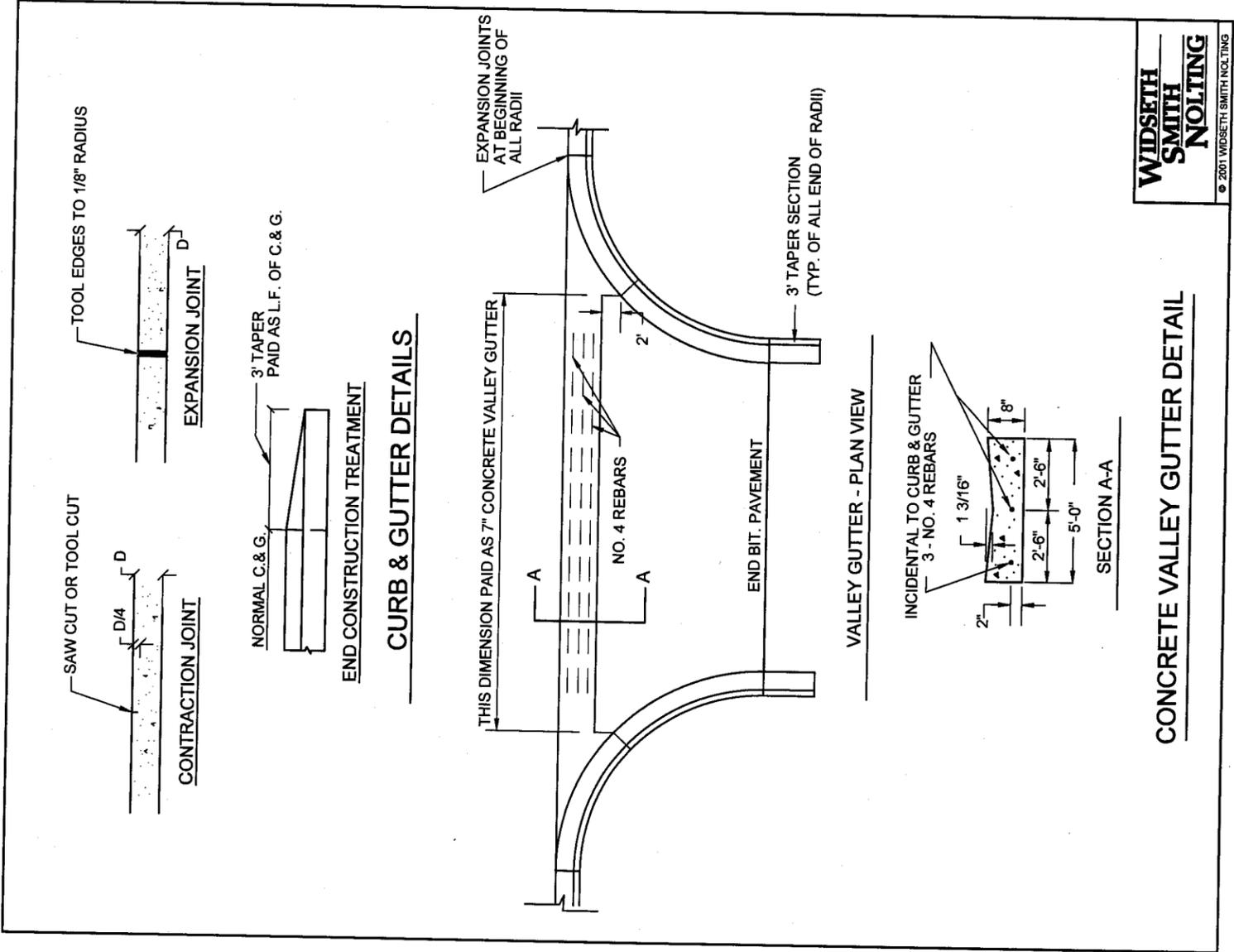
DATE	REV#	REVISION DESCRIPTION

BY: \_\_\_\_\_  
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.



WIDSETH SMITH NOLTING  
Engineering | Architecture | Surveying | Environmental

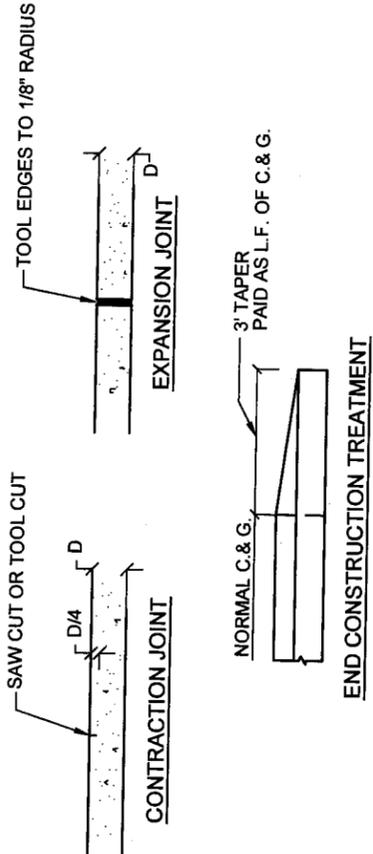
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**WIDSETH SMITH NOLTING**  
© 2001 WIDSETH SMITH NOLTING

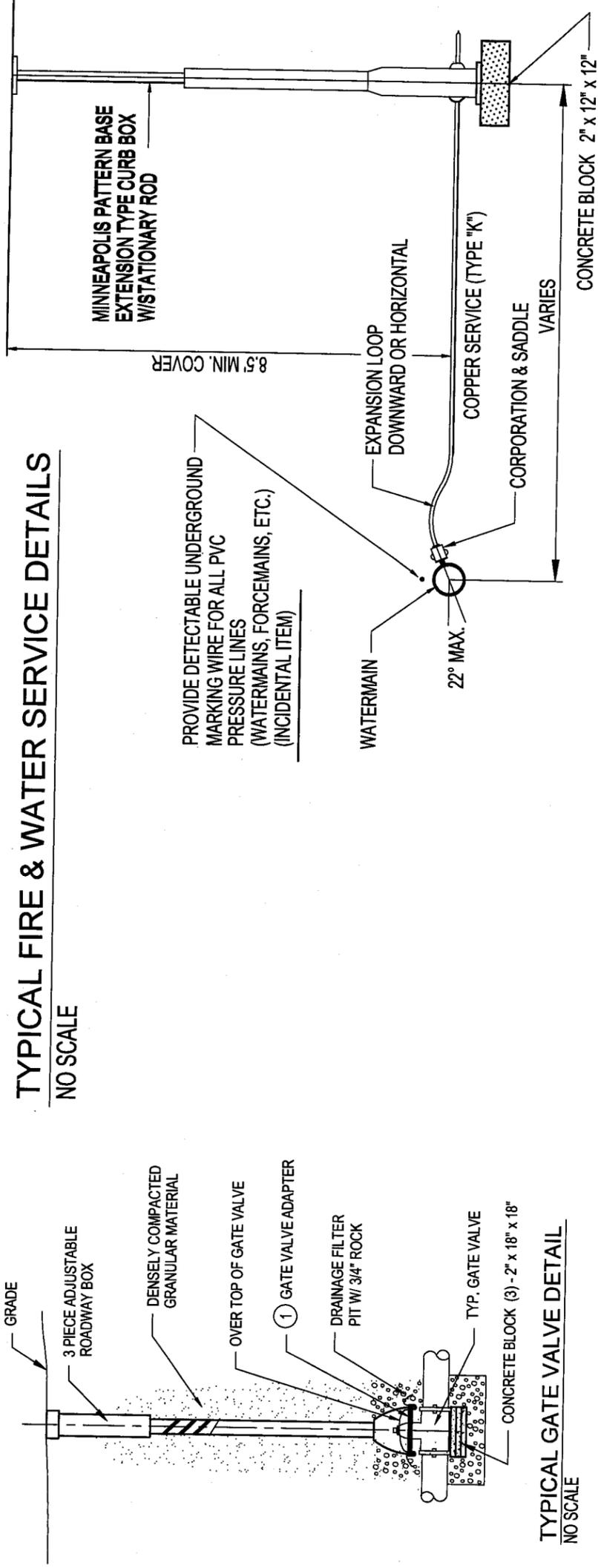
## CONCRETE VALLEY GUTTER DETAIL

### CURB & GUTTER DETAILS



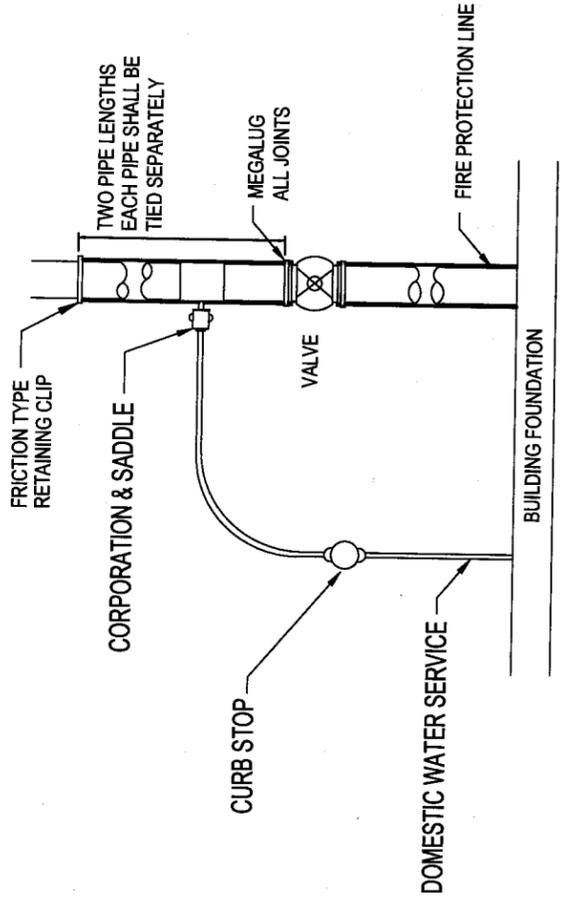
### TYPICAL FIRE & WATER SERVICE DETAILS

NO SCALE



TYPICAL GATE VALVE DETAIL  
NO SCALE

TYPICAL WATER SERVICE DETAIL  
NO SCALE



TYPICAL VALVE DETAIL 2" OR LESS  
NO SCALE

**NOTES:**

- ① ALL VALVE BOXES SHALL BE INSTALLED UPON THE GATE VALVE WITH THE USE OF A GATE VALVE ADAPTER AS MANUFACTURED BY ADAPTER INC. OR APPROVED EQUAL. THE GATE VALVE ADAPTER SHALL BE INCIDENTAL TO VALVE AND BOX INSTALLATION

PRELIMINARY - NOT FOR CONSTRUCTION

NORTHWOODS CROSSING  
BARATO BROTHERS CONSTRUCTION  
BAXTER, MINNESOTA  
CONSTRUCTION DETAIL

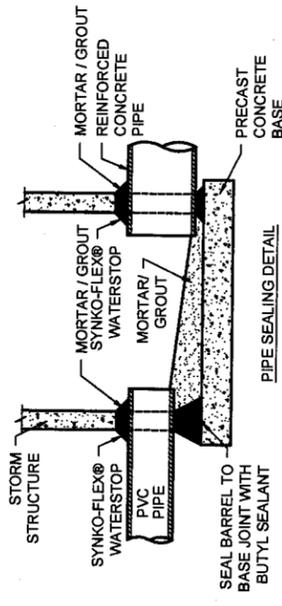
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SCALE: AS SHOWN  
DRAWN BY: METALS  
CHECKED BY: MER  
JOB NUMBER: 028780055.000

DATE	REVISION	DESCRIPTION

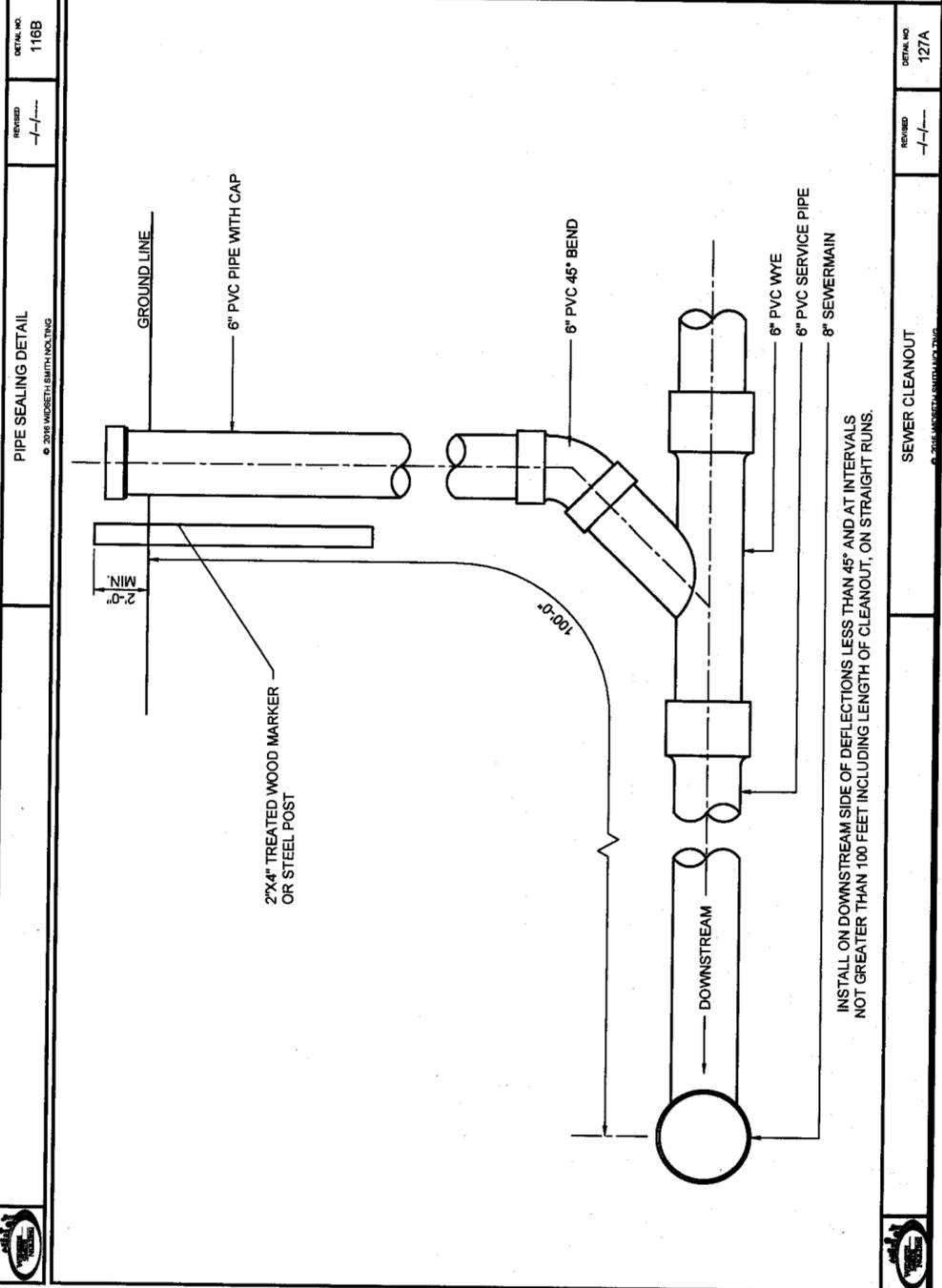
BY: MICHAEL E. RUDE  
DATE: MARCH 2016  
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.



# PRELIMINARY - NOT FOR CONSTRUCTION



NOTE: ALL MORTAR / GROUT SHALL BE PROVIDED BY A CERTIFIED CONCRETE FACILITY. NO FIELD MIXING OF MORTAR / GROUT WILL BE ACCEPTED.



PIPE SEALING DETAIL  
© 2016 WIDSETH SMITH NOLTING  
DETAIL NO. 116B

SEWER CLEANOUT  
© 2016 WIDSETH SMITH NOLTING  
DETAIL NO. 127A

# PRELIMINARY - NOT FOR CONSTRUCTION

ITEM	SYM	COMMON NAME	BOTANICAL NAME	QTY	MIN SIZE	MAX SIZE	ROOT
Overstory Trees							
	ABS	Autumn Brilliance Serviceberry	Amelanchier x grandiflora 'Autumn Brilliance'	7	2' Cal	H: 20-25' W: 15'	Ball
	GC	Gladiator Crabapple	Malus x adstringens 'Durelo'	7	2' Cal	H: 20' W: 9'	Ball
Coniferous Trees							
	ERC	Eastern Red Cedar	Juniperus virginiana	6	6' Height	H: 30-50' W: 8-20'	Ball
Shrubs							
	GBG	Gold Heart Bleeding Heart	Diemna spectabilis 'Gold Heart'	3	3 Gal	H: 24-30' W: 24-36'	Cont.
	MKL	Miss Kim Lilac	Syringa patula 'Miss Kim'	16	3 Gal	H: 6-7' W: 5-6'	Cont.
	NGF	Norham Gold Forsythia	Forsythia 'Norham Gold'	8	3 Gal	H: 6-8' W: 5-7'	Cont.

### LANDSCAPE PLANTING REQUIREMENTS:

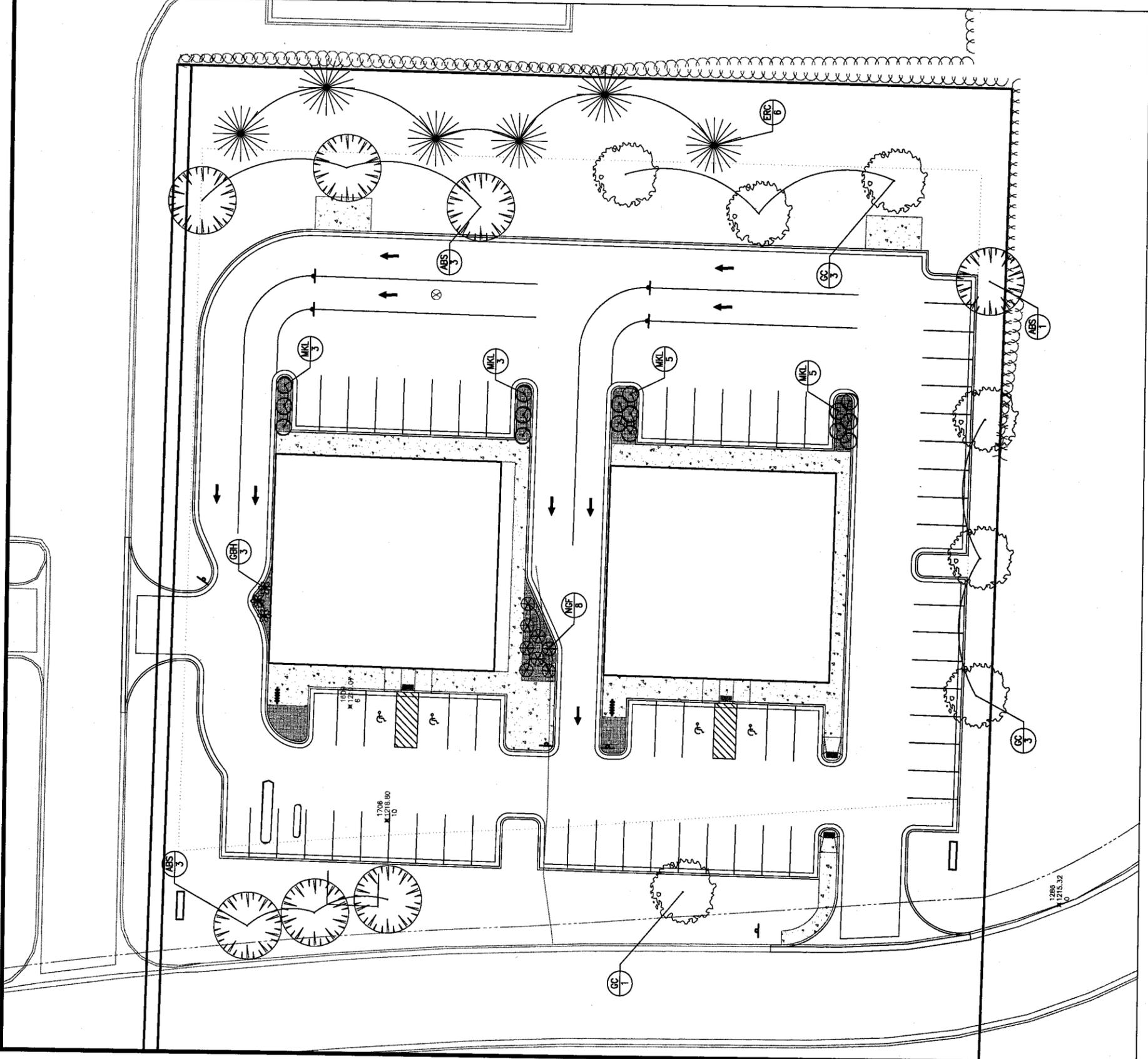
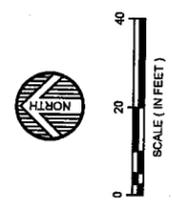
#### BASE REQUIREMENTS:

- OVERSTORY TREES
- 1 TREE/ 75' OF STREET FRONTAGE  
-301' OF STREET FRONTAGE = 4 TREES
  - 1 TREE/ 75' SITE PERIMETER  
-1,234 FT = 16 TREES
  - 1 SHRUB/ 45' OF SITE PERIMETER  
-1,234 FT = 27 SHRUBS

TOTAL TREES REQUIRED = 20  
30% OF TREES MUST BE CONIFEROUS = 6  
TOTAL TREES PROPOSED = 20  
TOTAL SHRUBS PROPOSED = 27

### SYMBOLS LEGEND

- SHADE TREE: GLADIATOR CRABAPPLE
- SHADE TREE: AUTUMN BRILLIANCE SERVICE BERRY
- CONIFEROUS TREE: EASTERN RED CEDAR
- RIVER ROCK: 1 1/2" - 2" STONES
- SHRUB: MISS KIM LILAC
- SHRUB: NORTHERN GOLD FORSYTHIA
- SHRUB: GOLD HEART BLEEDING HEART
- PLANT SYMBOL (SEE SCHEDULE)
- QUANTITY, THIS LOCATION

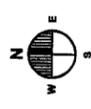
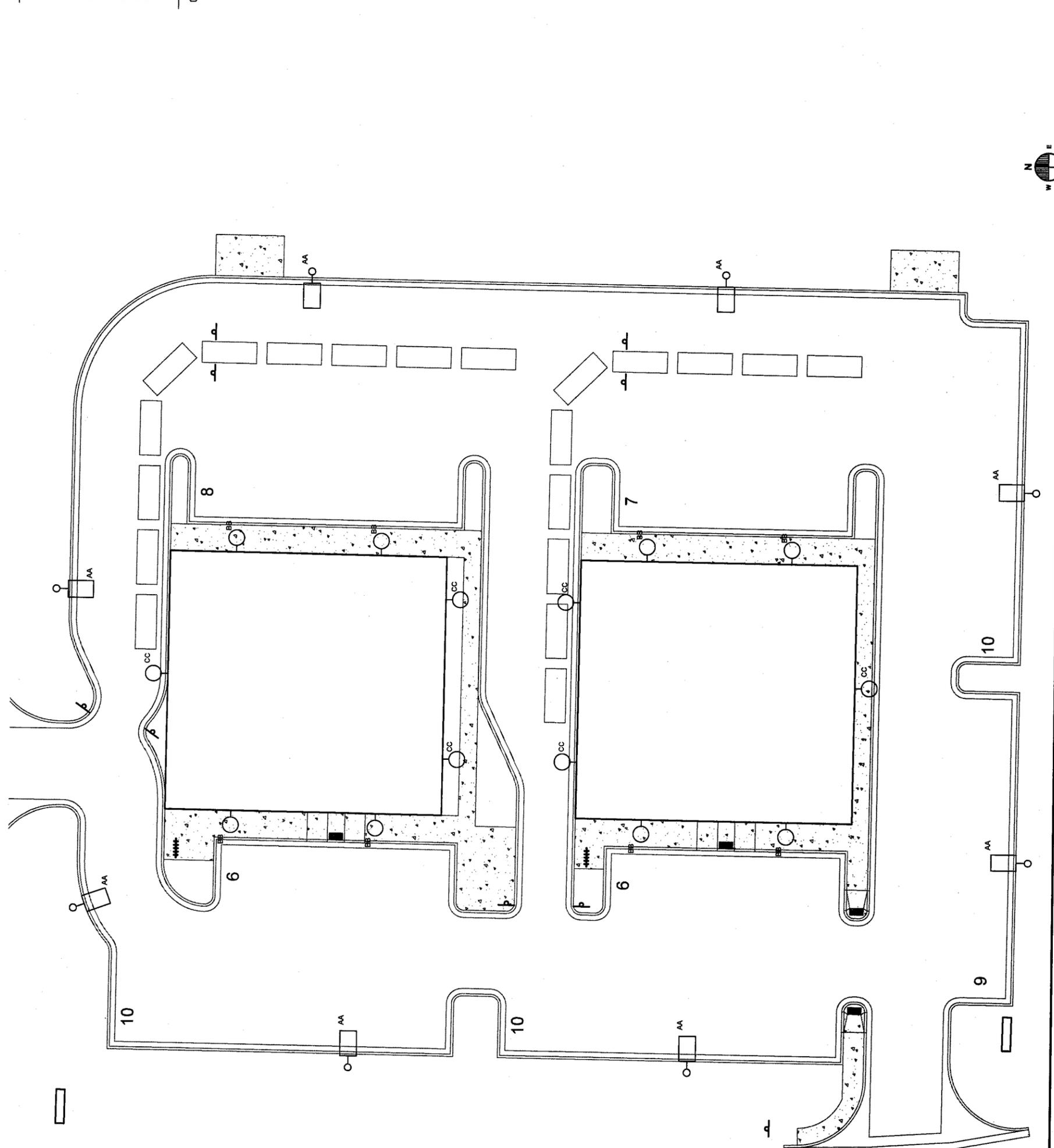




DATE	REV#	REVISIONS DESCRIPTION

PRELIMINARY DOCUMENTS

- GENERAL ELECTRICAL NOTES:**
- ALL ELECTRICAL WORK SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE NATIONAL ELECTRICAL CODE AND ALL STATE AND LOCAL CODES AND ORDINANCES.
  - IT IS THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE THE ELECTRICAL INSTALLATION REQUIREMENTS FOR THE ELECTRICAL SERVICE WITH THE UTILITY COMPANY.
  - ALL RACEWAYS BELOW GRADE SHALL BE MINIMUM OF 3/4" PVC CONDUIT UNLESS NOTED OTHERWISE. CONDUCTORS SHALL BE MINIMUM #10 AWG UNLESS NOTED OTHERWISE.
  - ALL UNDERGROUND CONDUIT SHALL BE BURIED A MINIMUM OF 24" BELOW FINISHED GRADE.
  - FIXTURE AA MOUNTED ON SQUARE POLE AT 20FT
  - FIXTURES BB AND CC MOUNTED ON BUILDING AT 12FT
- ELECTRICAL REFERENCE NOTES:**

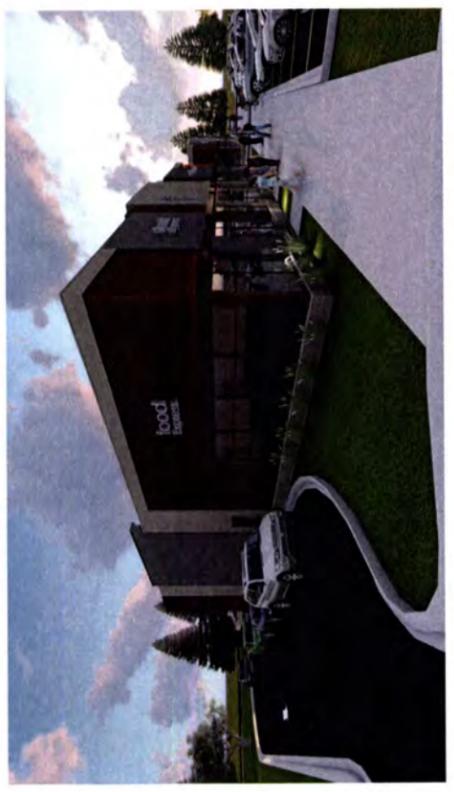


1 SITE PLAN  
 11-1-2020

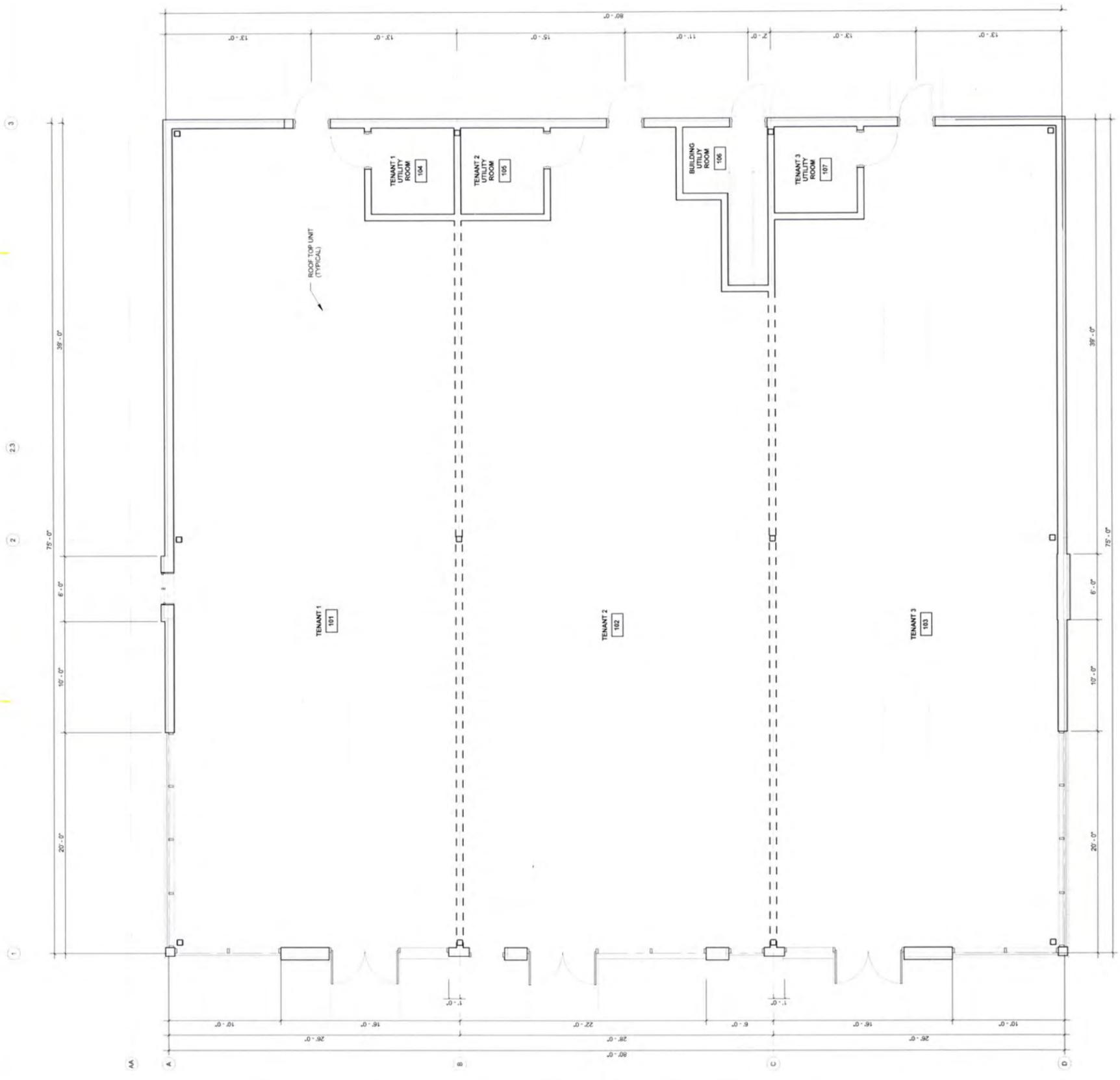
DATE:	OCTOBER 2016
SCALE:	3/16" = 1'-0"
BRAIN BY:	AGM
CHECKED BY:	DAM
JOB NUMBER:	287B0055.000

DATE REV# REVISIONS DESCRIPTION BY  
 HERBERY CENTRY THAT THIS PLAN, SPECIFICATION OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA  
 DOUGLAS A. ARCHER SIGN DATE LIC# 22902

# NFC



1 FLOOR PLAN  
3/16" = 1'-0"



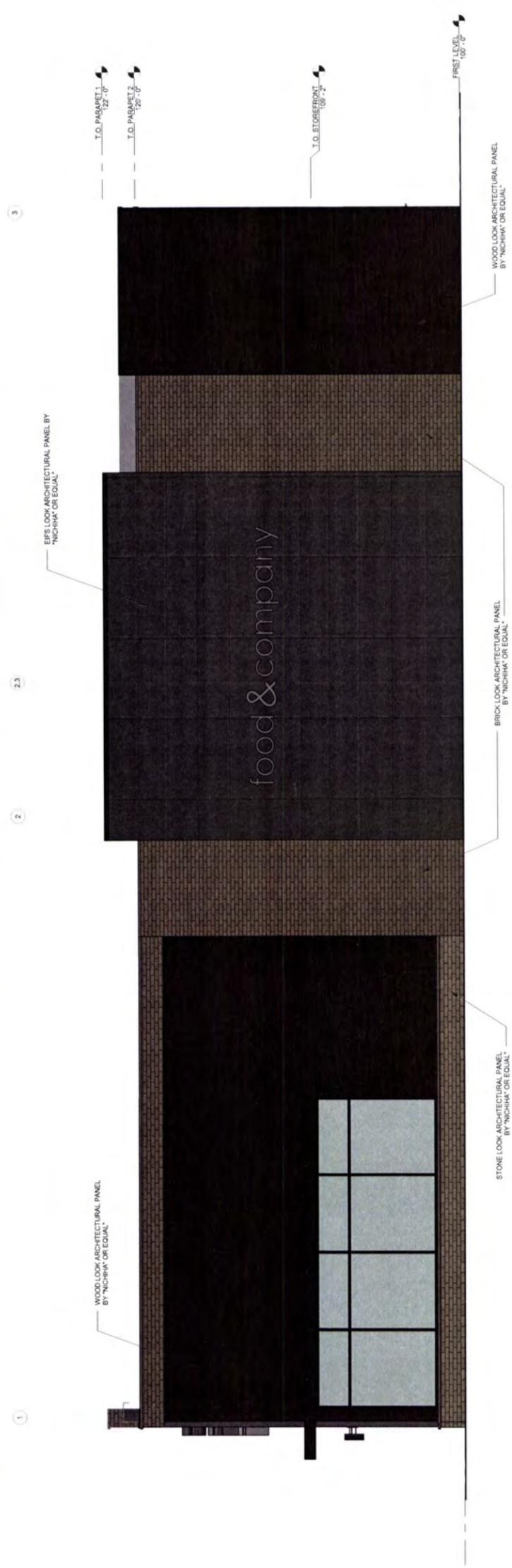


STATE OF MINNESOTA  
 LICENSED ARCHITECT UNDER THE LAWS OF THE  
 DIRECT SUPERVISION AND THAT I AM A DULY  
 REGISTERED ARCHITECT UNDER THE LAWS OF THE  
 STATE OF MINNESOTA  
 I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION,  
 AND REPORT WAS PREPARED BY ME OR UNDER MY  
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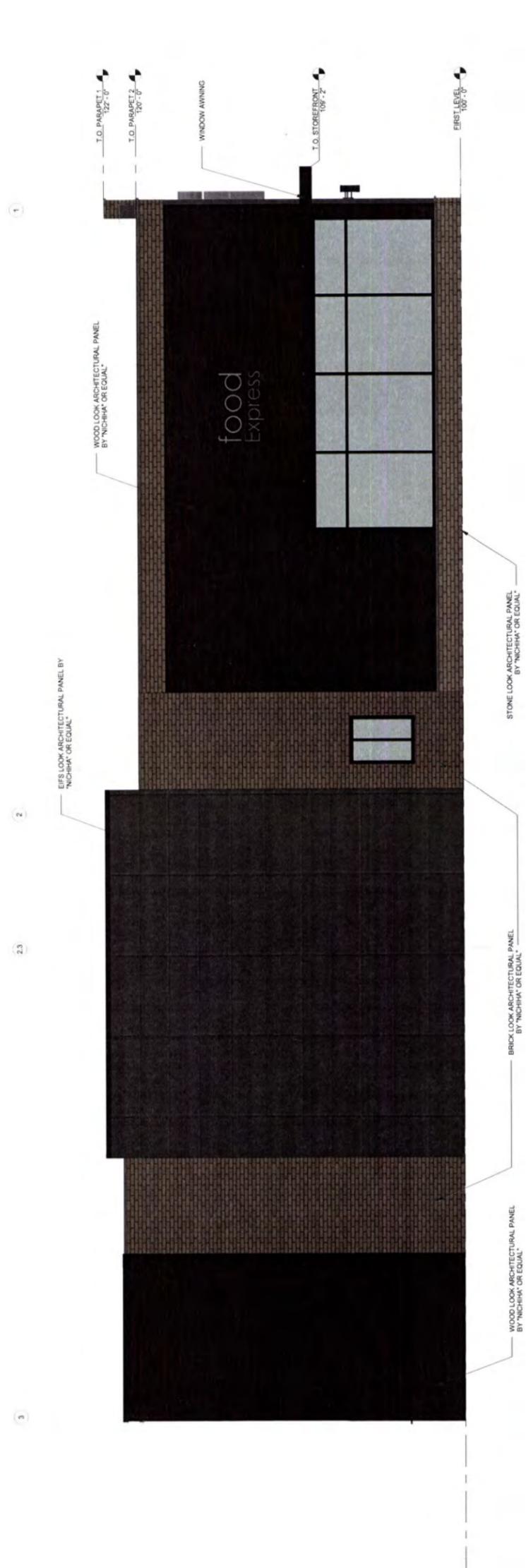
DATE: REV# REVISIONS DESCRIPTION BY  
 OCTOBER 2016  
 SCALE: 1/4" = 1'-0"  
 DRAWN BY: Author  
 CHECKED BY: Checker  
 JOB NUMBER: 287B0055.000

371 RETAIL  
 BAXTER, MINNESOTA  
 BUILDING ELEVATIONS

**NFC**



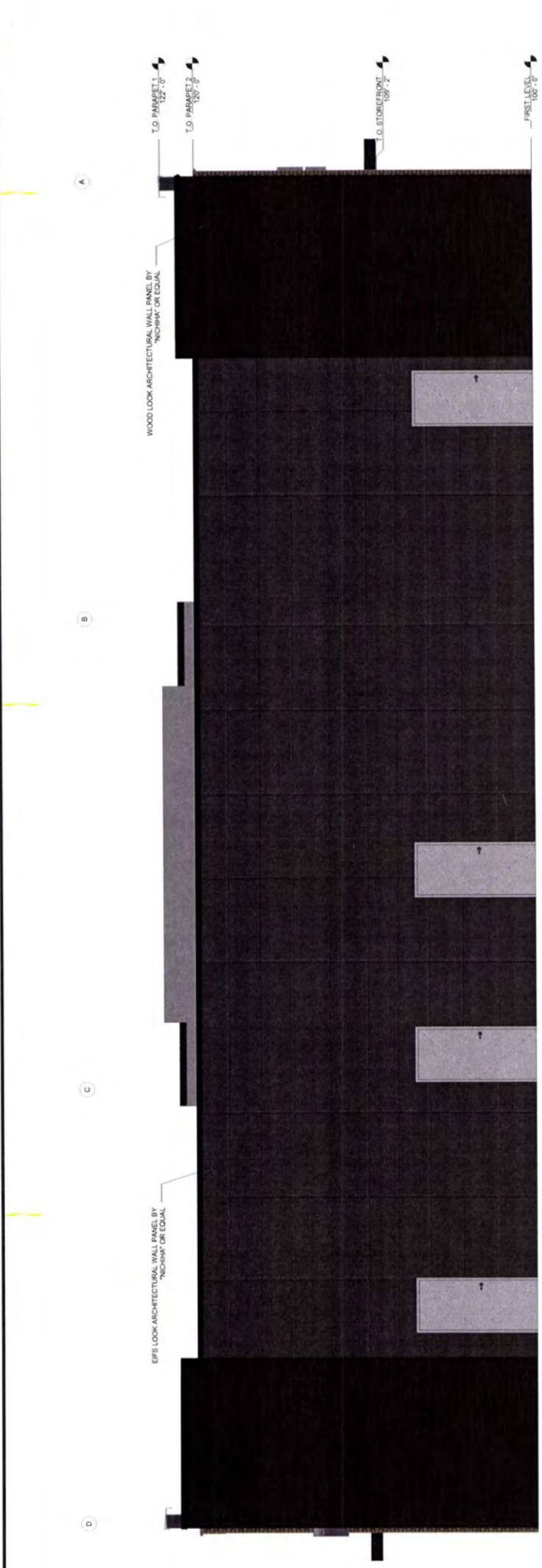
1 SOUTH ELEVATION  
 1/4" = 1'-0"



2 NORTH ELEVATION  
 1/4" = 1'-0"

DATE:	OCTOBER 2016
SCALE:	1/4" = 1'-0"
DRAWN BY:	AJW
CHECKED BY:	CHKM
JOB NUMBER:	287B0055.000

DATE: REV# REVISIONS DESCRIPTION BY  
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1 EAST ELEVATION  
1/4" = 1'-0"



2 WEST ELEVATION  
1/4" = 1'-0"